

BRIDGES OF COUNTY LAOIS: AN INDUSTRIAL HERITAGE REVIEW PART 2



Site Gazetteer: Volume 1 North Laois

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*An Action of the
Laois Heritage Plan 2007 – 2011*



for
Laois County Council
March 2009



Cover: Dunrally Bridge over the River Barrow, Vicarstown (LAIAR-014-010).

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PREFACE

This volume contains a gazetteer of bridges in north Co Laois, specifically those on Ordnance Survey six-inch maps 1-14 inclusive. The remaining sites are itemised Volume 2. A general site overview has already been presented in Part 1 of this report.

Details of the location, function, history, upstanding remains and heritage merit of each bridge are presented in the gazetteer. It is preceded by an explanation of the layout of each site report and is succeeded by indexes to enable the reader to find specific sites.

The report is accompanied by a DVD which reiterates the texts of both Parts 1 and 2 in PDF format. This disc also contains an *Access* database of all the bridges included in the gazetteer, a *MapInfo* database of their locations, and photographs in JPEG format. The appropriate computer software will be necessary in order to utilize the databases. However, a free PDF reader - *Adobe Acrobat* - is downloadable on the internet and will also enable the user to search for any text within both parts of the report without recourse to the indexes.

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INTRODUCTION

All the data collected during the course of this project has been transferred to the Laois Industrial Archaeology Record (LAIAR). This is in the form of a Microsoft Access database and contains records for a wide variety of industrial sites throughout the county. Each site is uniquely identified by its LAIAR number and has a corresponding record in the database. Each record contains information on the site's location, history, physical remains and heritage significance. Selected data for all sites are also held in *MapInfo* format and can be superimposed on maps at various scales.

Site gazetteer fields

The key features of each site are printed out in hard-copy format in this volume. The fields within each site record are as follows:

Site number	This is the Laois Industrial Archaeology Record number (LAIAR). It comprises two sets of three digits. The first set denotes the Ordnance Survey County Series six-inch map on which the site is depicted. The second set denotes the number of the site within that particular sheet. For example LAIAR-029-050 denotes site 50 on sheet 29 of the Co Laois six-inch maps.
Name	This is the name by which the site is officially or commonly known (usually derived from the OS maps). Bridges may sometimes have several different names, each of which is listed (where known). Defunct names are given in curly brackets.
County	The county within which the site lies, here Co Laois (previously Queen's County). Where a bridge straddles a county boundary, the neighbouring counties are also noted.
Townland	This is the townland(s) where the site is located. As townland boundaries are often along rivers, many bridges will be located in several townlands. These are listed alphabetically.
Town	This is given in those instances where the site is located within an urban area.
Planning	This is the Planning Authority responsible for that site. Where a bridge is shared with an adjoining county, the neighbouring Authority is also listed.
Discovery map	OS 1:50,000 Discovery Series map sheet.
Six-inch map	OS 1:10,560 County Series map sheet.
Summary	This briefly outlines the bridge's function, historical development and what now remains.
History	This is an overview of the site's development based primarily on depictions on past editions of the OS six-inch maps, supplemented by published information where available.

Each site component is now detailed:

Component	LAIAR site number followed by component number, e.g. LAIAR-003-011 2 is component 2 at site 003-011. Many sites will comprise a single bridge but some may have several on the one spot, for example where there has been a replacement. Each bridge is regarded as a component of that site and is accorded its own number within the overall site number. There are 428 bridge sites and 477 components. Further details of the numbering of site components are given in Part 1: Appendix 2.
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Type	<p>Bridges are classified according to what is carried over what, e.g. Bridge (foot/canal) is a footbridge over a canal. There are 16 such types in Co Laois:</p> <ul style="list-style-type: none"> ▪ Bridge (canal/river) [= aqueduct] ▪ Bridge (foot/canal) ▪ Bridge (foot/rail) ▪ Bridge (foot/river) ▪ Bridge (foot/road) ▪ Bridge (rail/canal feeder) ▪ Bridge (rail/canal) ▪ Bridge (rail/rail) ▪ Bridge (rail/river) ▪ Bridge (rail/road) ▪ Bridge (river/river) ▪ Bridge (road/canal feeder) ▪ Bridge (road/canal) ▪ Bridge (road/foot) ▪ Bridge (road/rail) ▪ Bridge (road/river)
Function	<p>Laois's bridges have one of four functional associations:</p> <ul style="list-style-type: none"> ▪ Coal mining ▪ Inland waterway ▪ Peat ▪ Railway ▪ Road & pedestrian <p>Road/pedestrian bridges were specifically erected as part of the road network, whereas those grouped under Inland waterway and Railway were built as a direct consequence of the construction of a canal or railway line. Bridges erected to transport coal and peat are grouped under Coal mining and Peat respectively.</p>
Category	<p>This field denotes the industrial category with which the bridge is associated. Laois's bridges fall into three categories:</p> <ul style="list-style-type: none"> ▪ Fuel & power production ▪ Mineral extraction, processing & products ▪ Transport <p>The vast majority of bridges are associated with Transport. However, those built by Bord na Mona for the conveyance of peat for horticultural use fall into the mineral extraction category and those built in association with the Wolfhill collieries under Fuel & power production.</p>
Context	<p>This field denotes the built heritage context within which the bridge operates. There are three such contexts:</p> <ul style="list-style-type: none"> ● Industry ● Infrastructure ● Settlement <p>Most bridges are integral to the county's public transport system and fall within the context of Infrastructure. Those built for coal and peat transportation come within Industry and those for private use within landed estates are under Settlement.</p>
Irish Grid	<p>This gives the component's easting and northing, each to six figures (i.e. to 1m).</p>
Accuracy	<p>This gives the accuracy to which the component has been plotted on the <i>MapInfo</i> base map. In most cases, the six-inch map has been used as the basis of the plot. These may differ slightly from those generated using modern base maps and aerial photos on account of their differing projections. The grid coordinates given for each</p>

	site component are reckoned to be within about 10m of their position on the ground.
Survey date	The day, month and year in which the site component was surveyed. A blank field denotes that it was not surveyed.
Surveyor	Name of surveyor (where surveyed).
Remains	This denotes the bridge's degree of survival: <ul style="list-style-type: none"> ▪ Complete = abutments and deck survive. ▪ Substantial remains = only abutments/piers survive. ▪ Some remains = not all abutments/piers survive. ▪ Traces = only basal remains now survive. ▪ No visible remains
Condition	This describes the condition of the remains: <ul style="list-style-type: none"> ▪ Excellent = virtually as new. ▪ Good = maintained in good condition. ▪ Fair = not maintained but in reasonable condition. ▪ Poor = not maintained and derelict. ▪ N/A = not applicable (in the case of sites with no visible remains).
Current use	The following uses were noted in the surveyed bridges: <ul style="list-style-type: none"> ▪ Aqueduct ▪ Culvert = where an active stream is carried under a defunct canal. ▪ Disused ▪ Foot bridge ▪ N/A = not applicable where nothing remains ▪ Rail bridge ▪ Road bridge
Description	A description of the physical elements which make up a particular component – span details, materials , condition etc.

The record returns to the overall site:

Interest	National Inventory of Architectural Heritage evaluation categories relevant to this particular site.
Evaluation	This is an evaluation of the industrial heritage significance of the overall site, including all its components.
Rating	This denotes whether the site is of no significance (Record only), or of Local, Regional or National significance.
Protection	This denotes whether the site has any form of statutory protection: RMP = Record of Monuments & Places RPS = Co Laois Record of Protected Structures
Action	Noted where the site is of regional or national significance. If the site is already protected, no action is required. Otherwise a recommendation is given for inclusion in the RPS and/or RMP.
Associated with	Other sites within the LAIAR database with which the site is associated (generally railways or canals).
Other dbase	Other databases containing additional details of this particular site. These databases are: <ul style="list-style-type: none"> ▪ IWAI MMC = Inland Waterways Assn Ireland Mountmellick Canal ▪ LA Bridge = Laois County Council Bridge ▪ Mountmellick Canal = Mountmellick Canal inventory (Laois County Council)

- NIAH = National Inventory Architectural Heritage
Heritage rating is given in brackets:
RO = record only; L = local; R = regional; N = national
- NRA Bridge = National Roads Authority Bridge
- OFIAR = Co Offaly Industrial Heritage Record (Offaly County Council)
- SMR = Sites & Monuments Record (Dept Env'ment, Heritage & Local Govt.)

References

This lists all published and unpublished references uncovered during the course of the 'paper survey' phase of the project. Most of these citations will be found in Laois County Library. Photocopies of the relevant passages wherein the bridge is cited are also to be found in the hard-copy LAIAR files.

Photographs

For each image the following details are given:

- Number: LAIAR-003-010_02 denotes image 3 of site-003-010.
- Photographer: initials of photographer (FWH = Fred Hamond)
- Date: day/ month/ year
- Caption: what the image shows

Site indexes

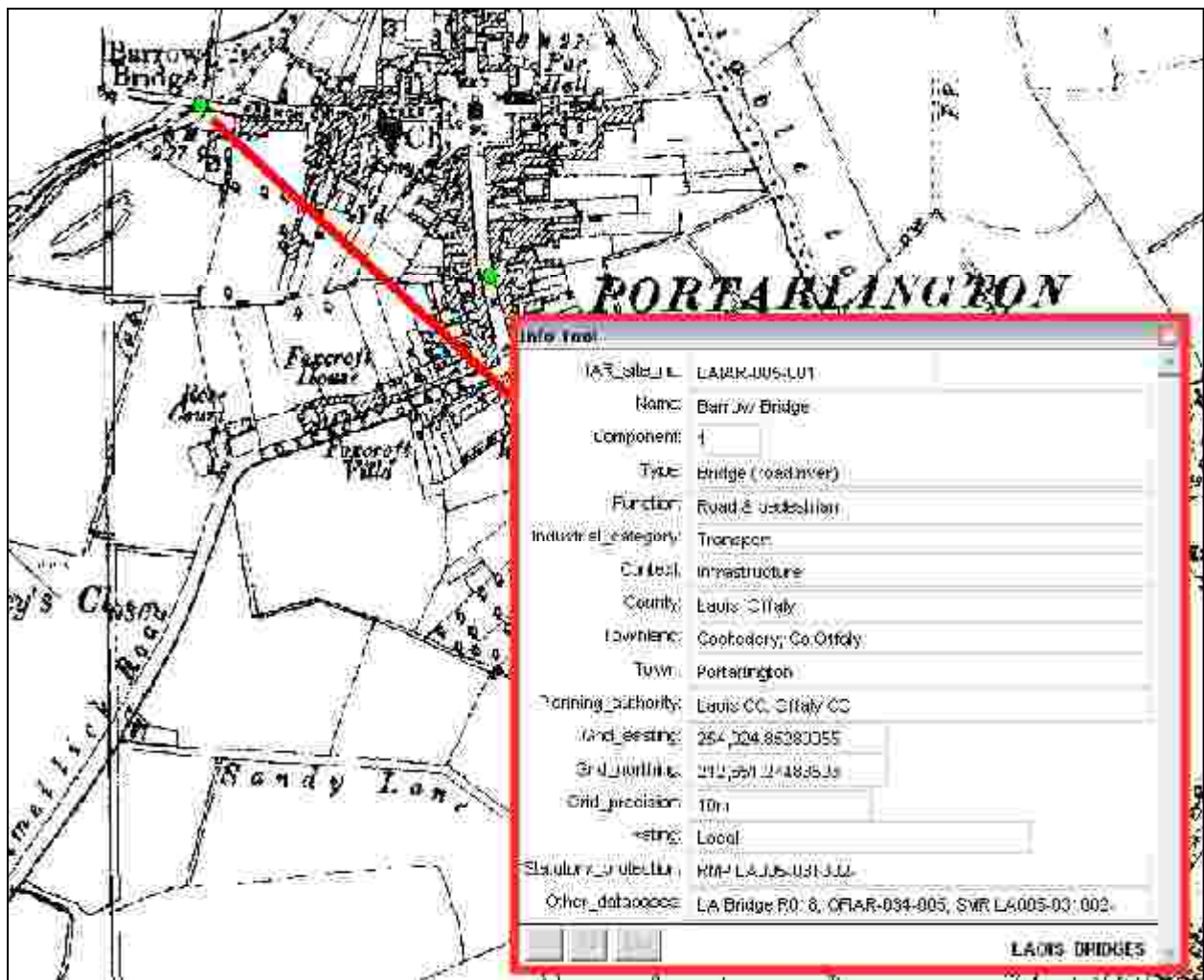
In order to find a particular site, or a group of sites which particular attributes, they have been indexed as follows:

1. Name – Type – Townland – Town -- LAIAR no
2. Townland – Town – Type – Name -- LAIAR no
3. Grid easting -- Grid northing – Type – Name -- LAIAR no
4. Type – Townland – Town – Name -- LAIAR no
5. Laois County Council Bridge no -- LAIAR no

MapInfo data

Selected data fields are also held in *MapInfo* format. This requires the user to have access to this particular software (*MapInfo Professional* version 7.5 was used here) as well as base maps over which to overlay the site distributions (e.g. OS Discovery and six-inch maps).

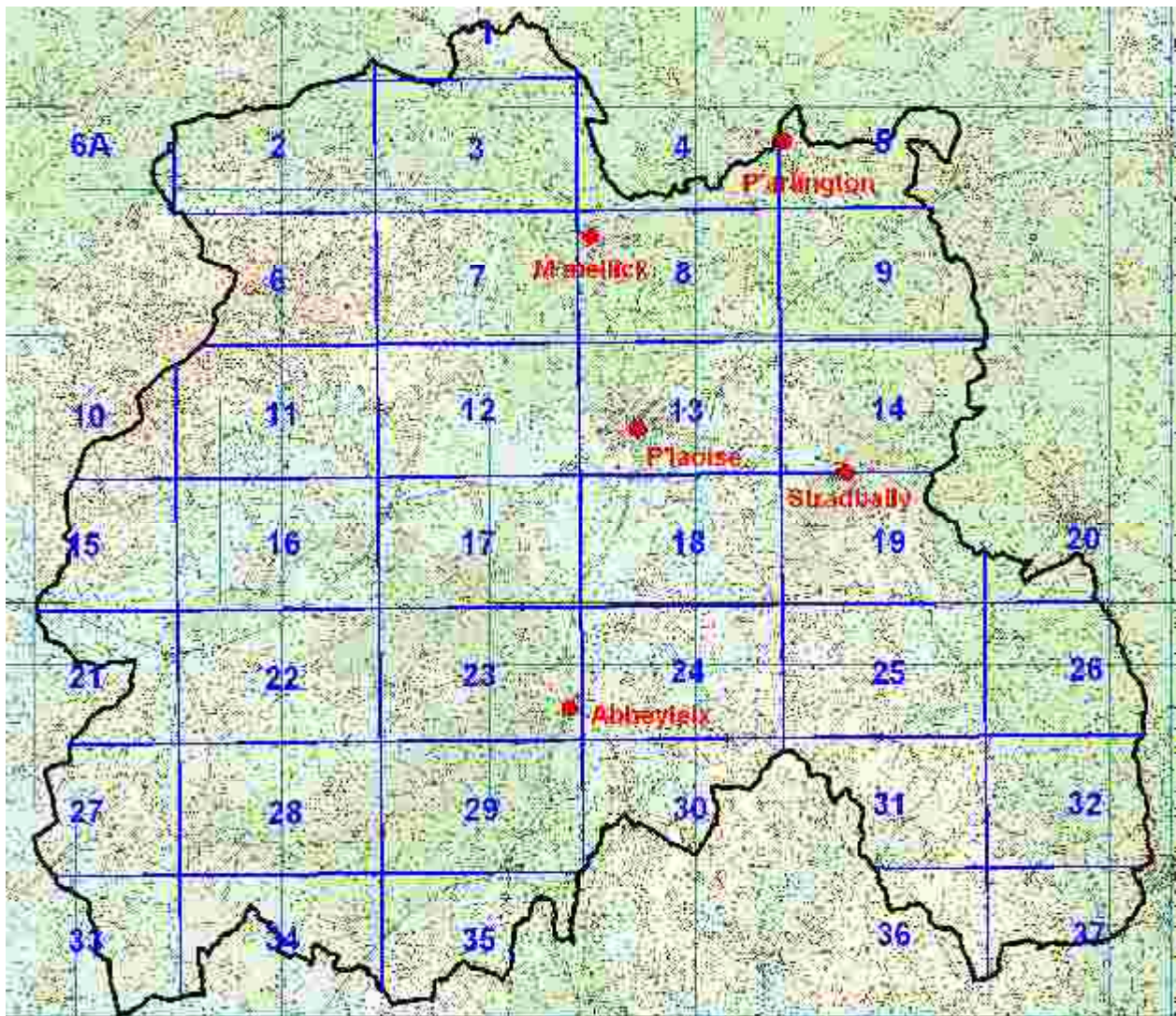
The following figure shows bridges in Portarlington superimposed on the OS six-inch map. The user has clicked the mouse over site LAIAR-005-001 to reveal the data relating to it. Additional details will also be found in the gazetteer contained in this volume.

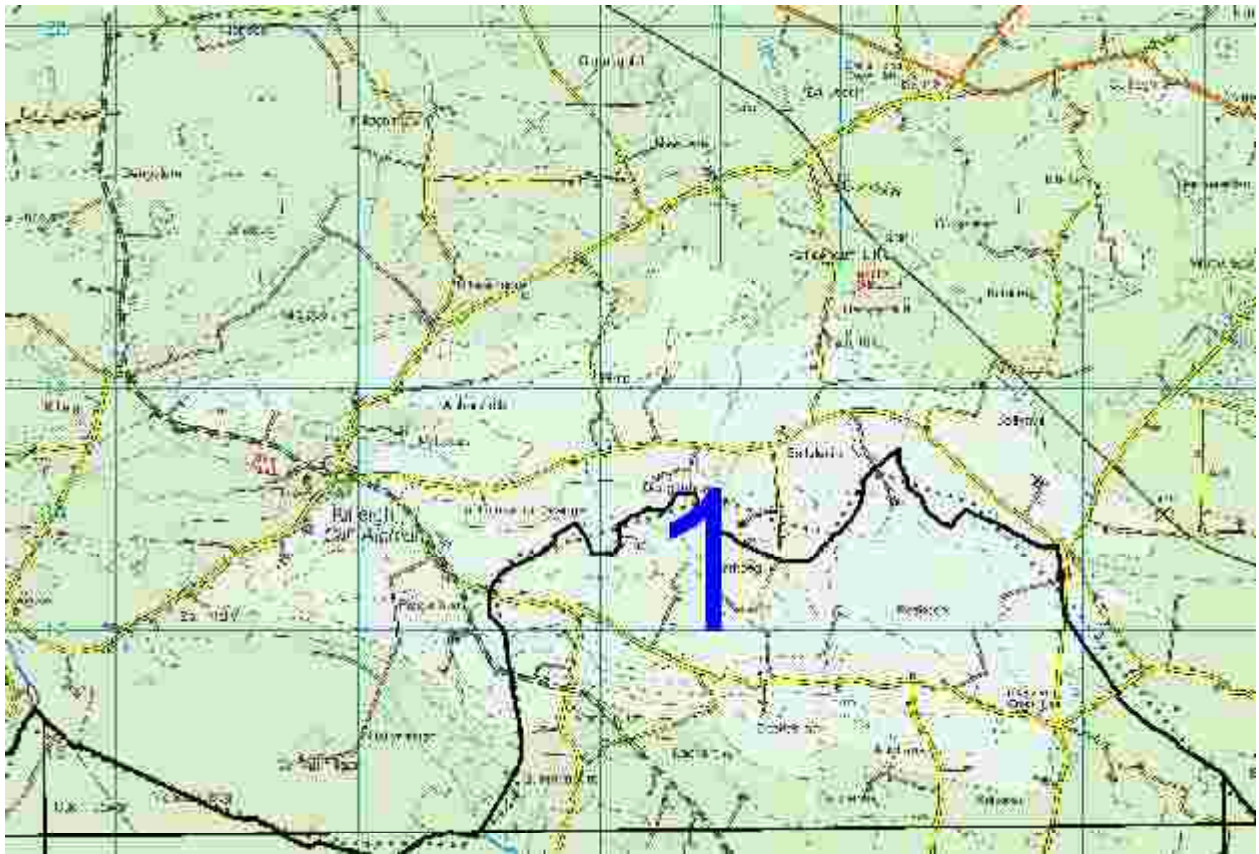


Example of map and data generated using *MapInfo* for bridges in the Portarlington area. Clicking on the arrowed site brings up the 'Info Tool' table which gives basic details that particular bridge.

BRIDGE LOCATION MAPS

Ordered by OS six-inch map sheets

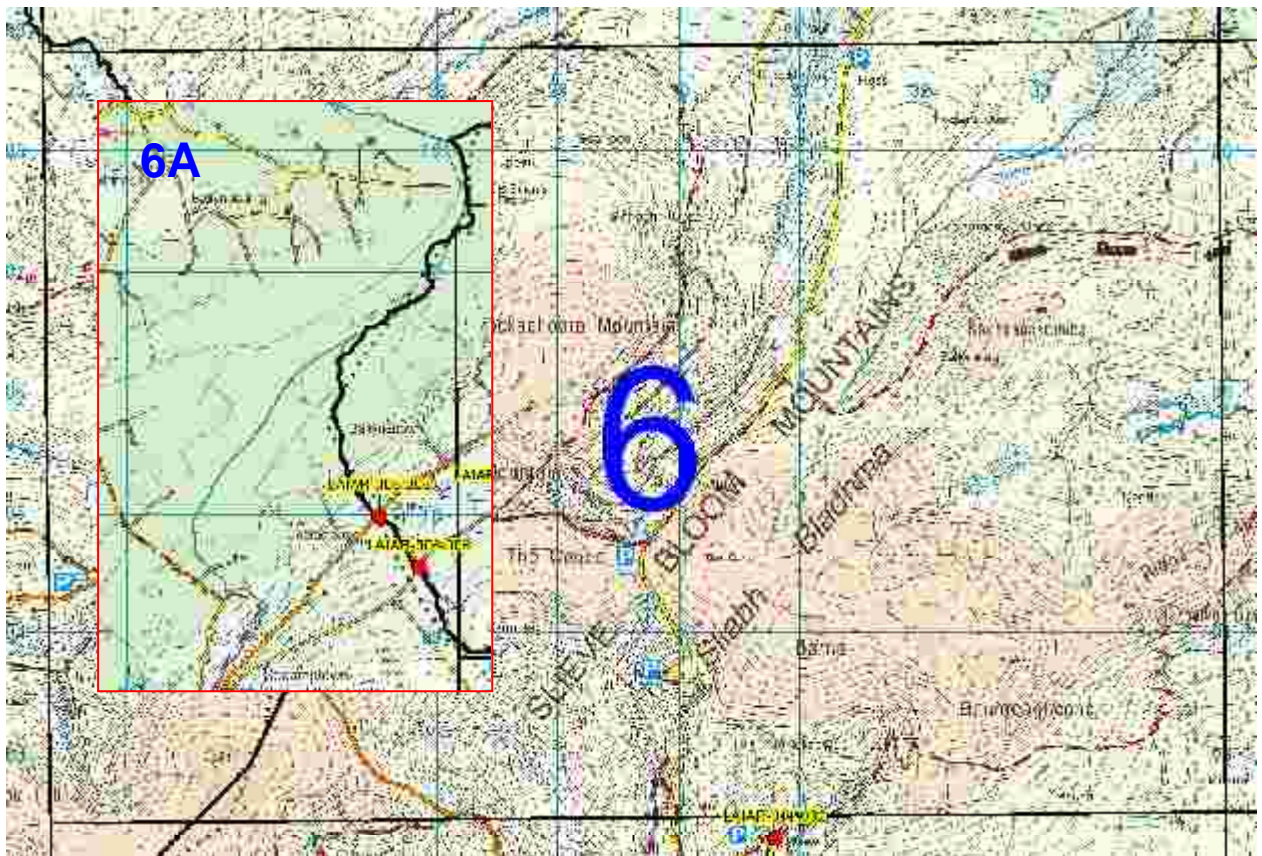


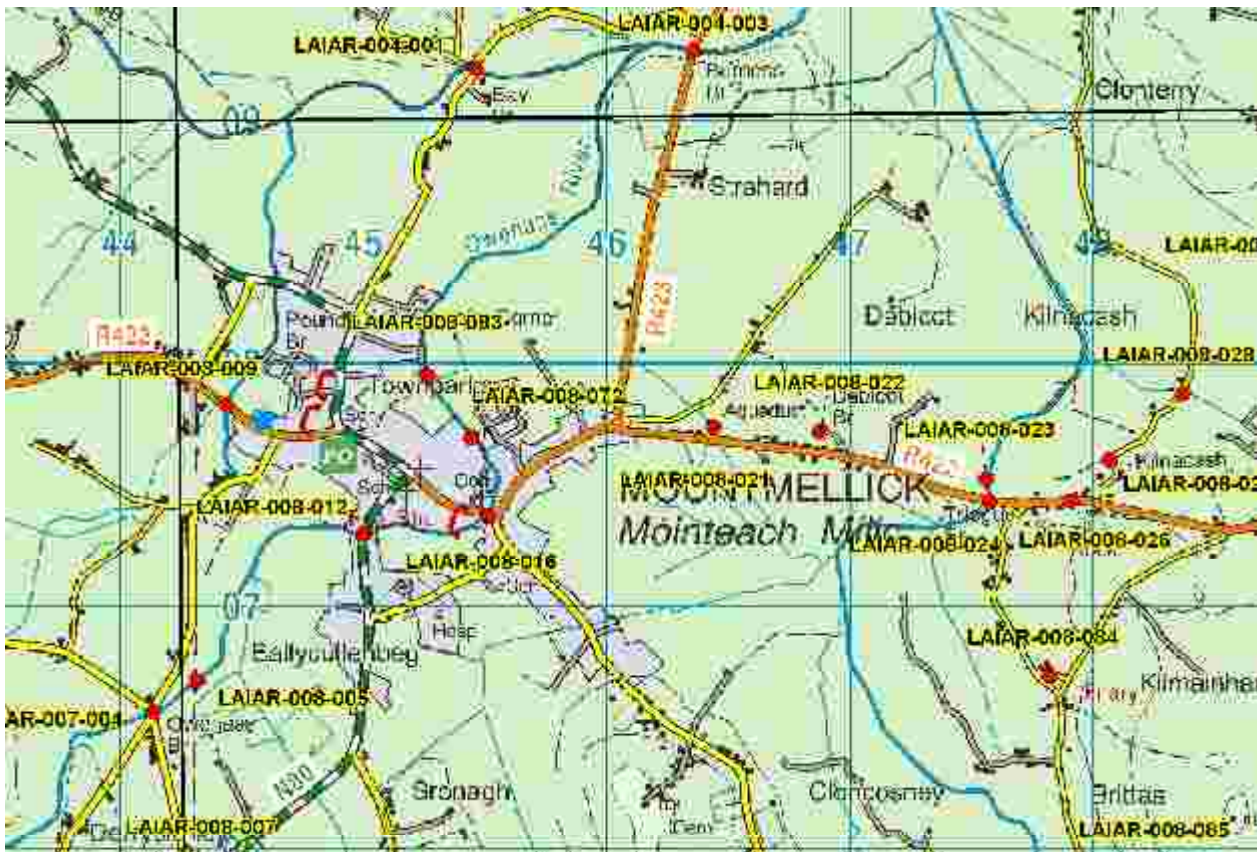




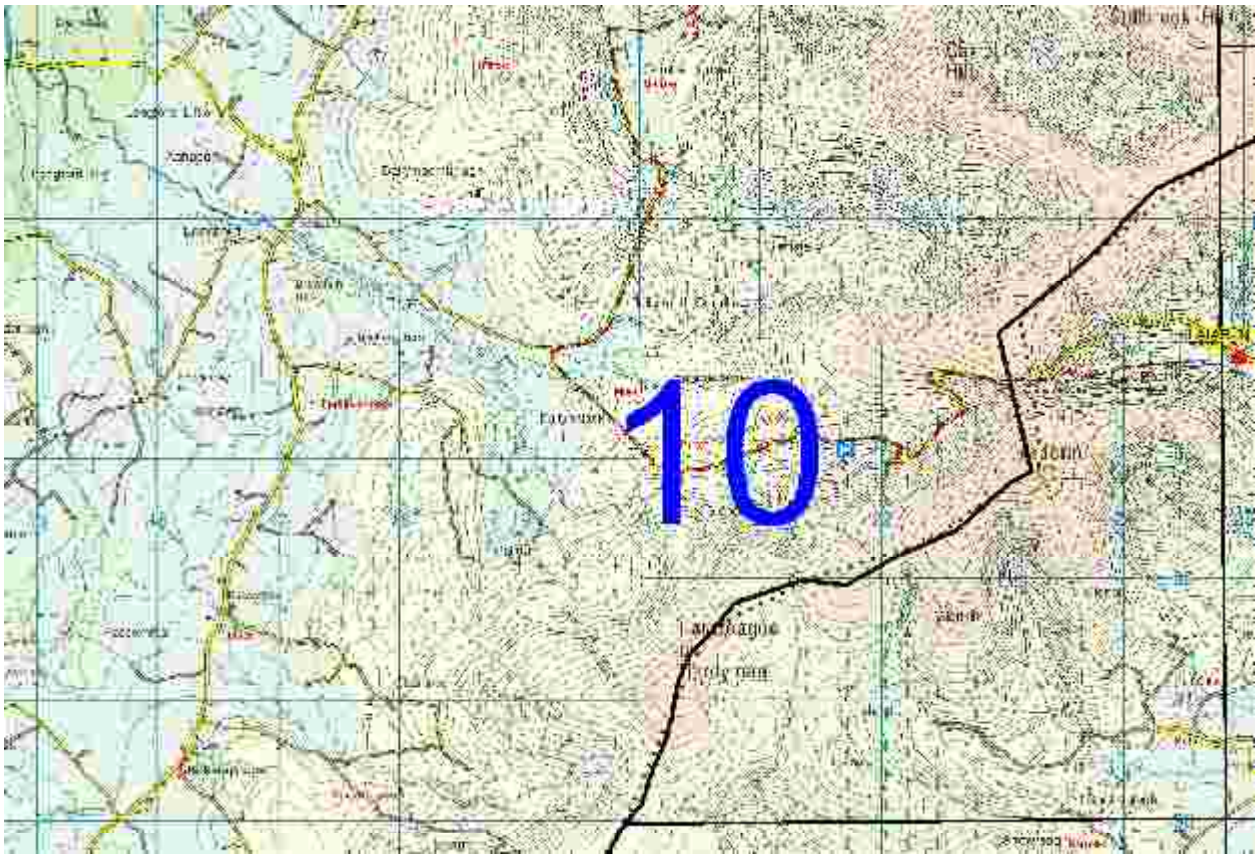


PORTARLINGTON

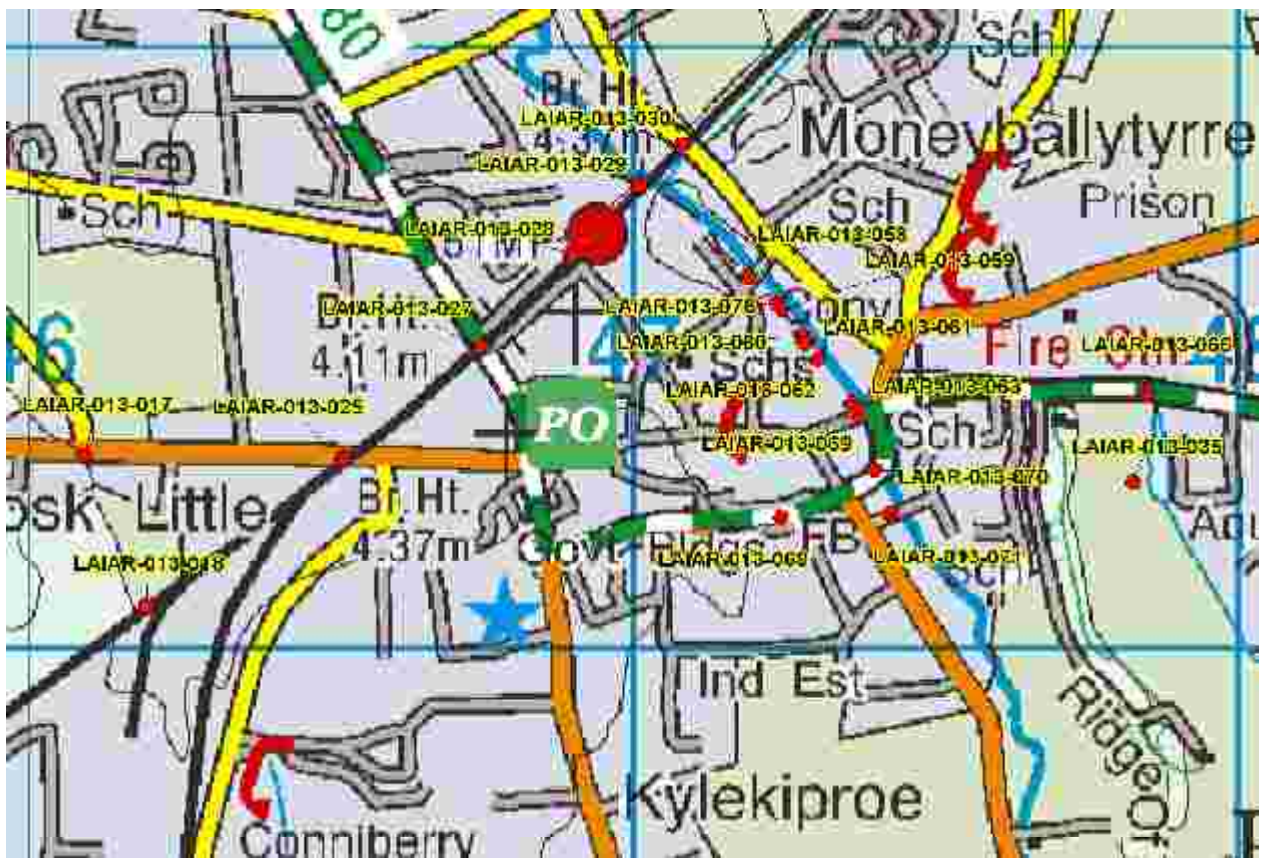




MOUNTMELICK



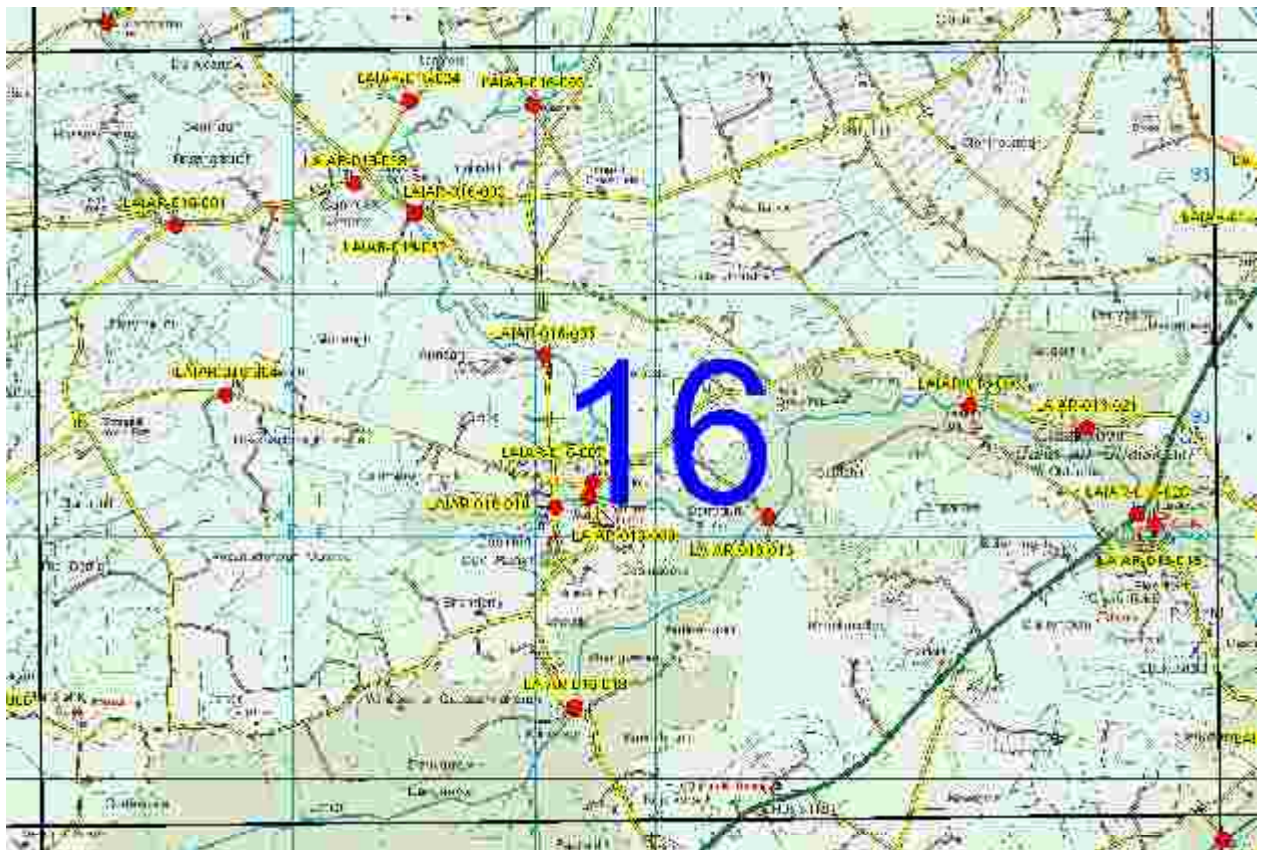
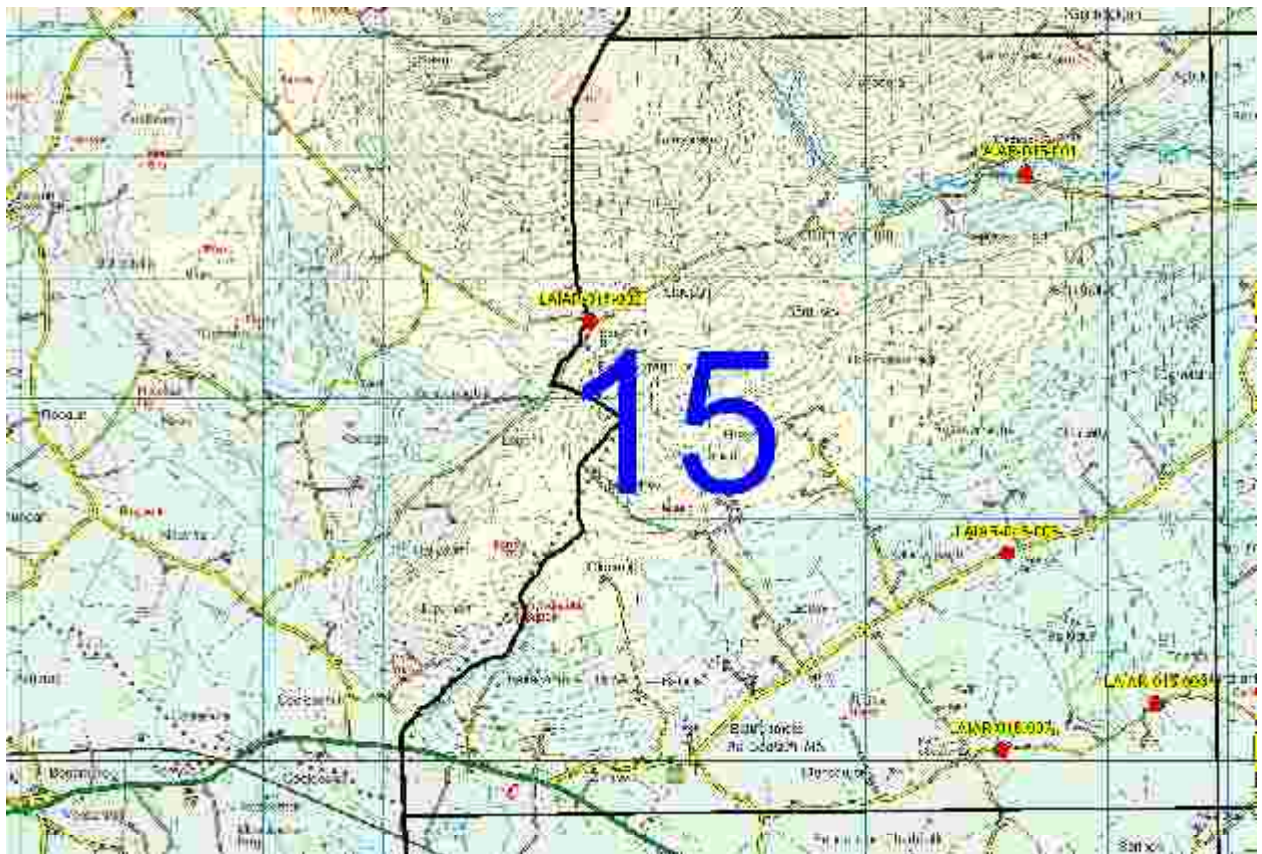




PORTLAOISE



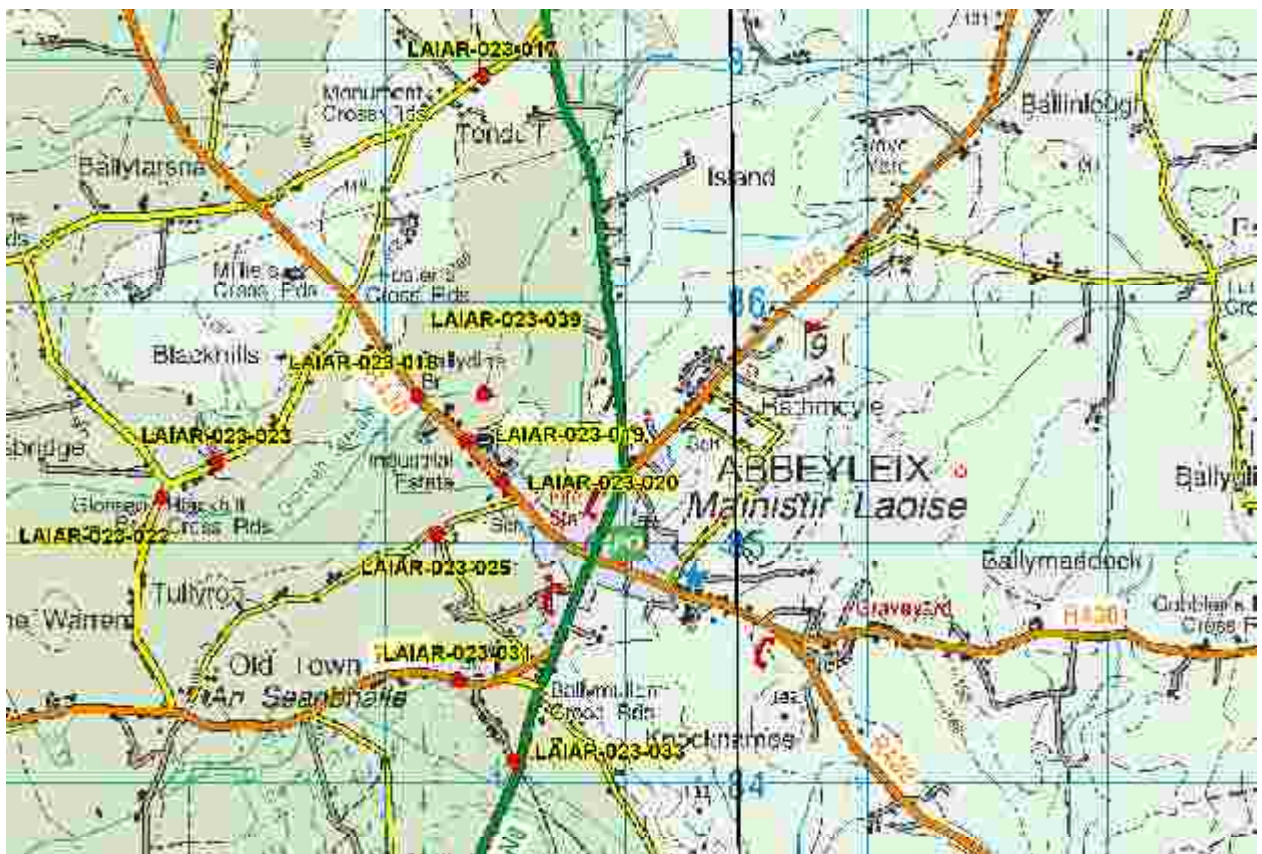
STRADBALLY





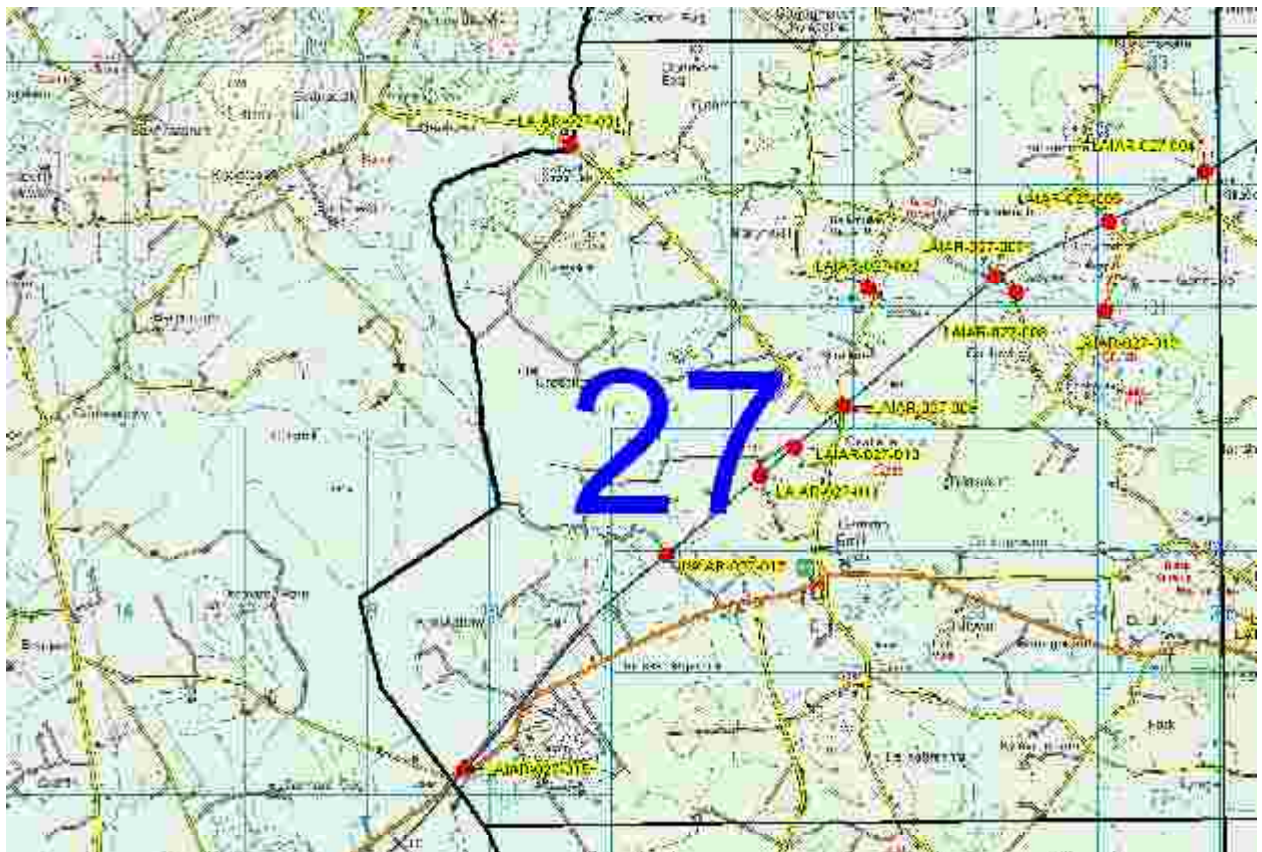




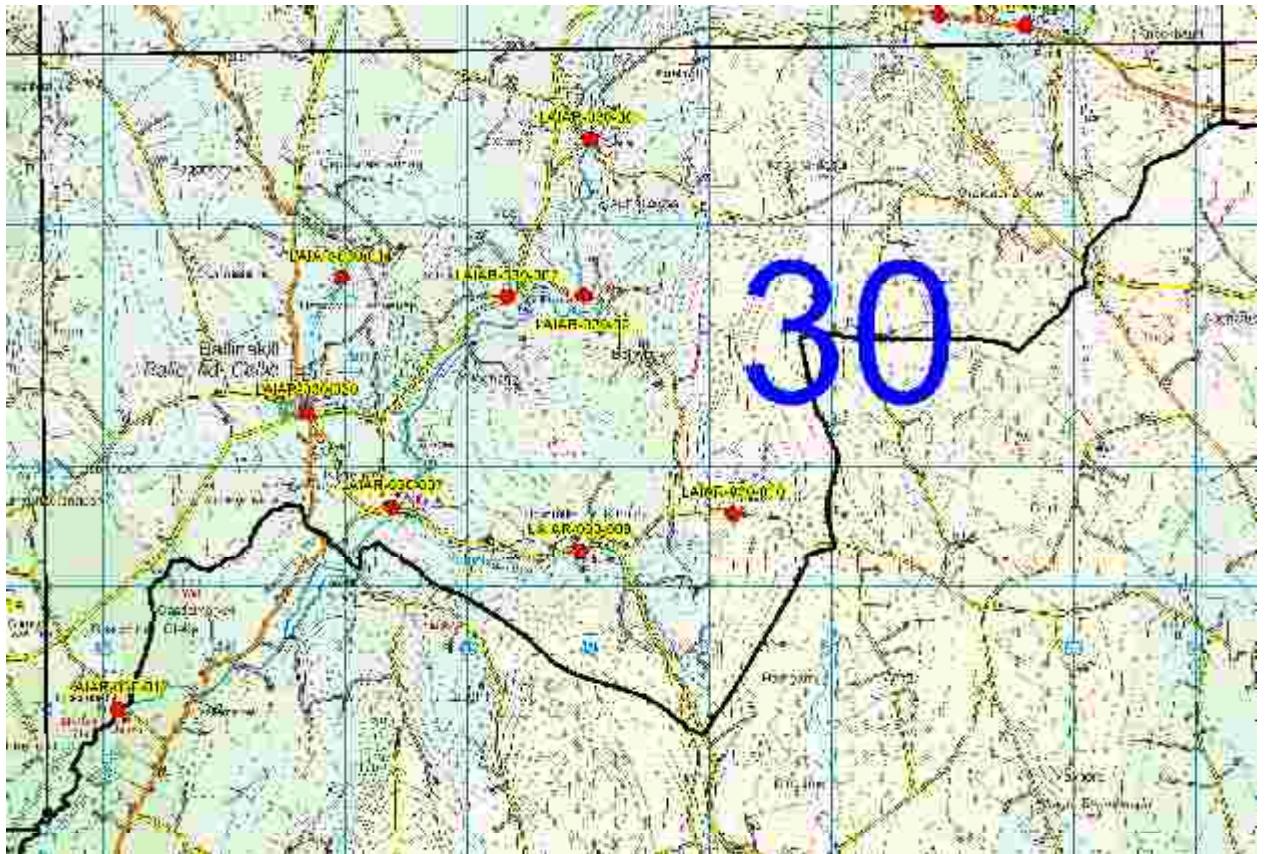


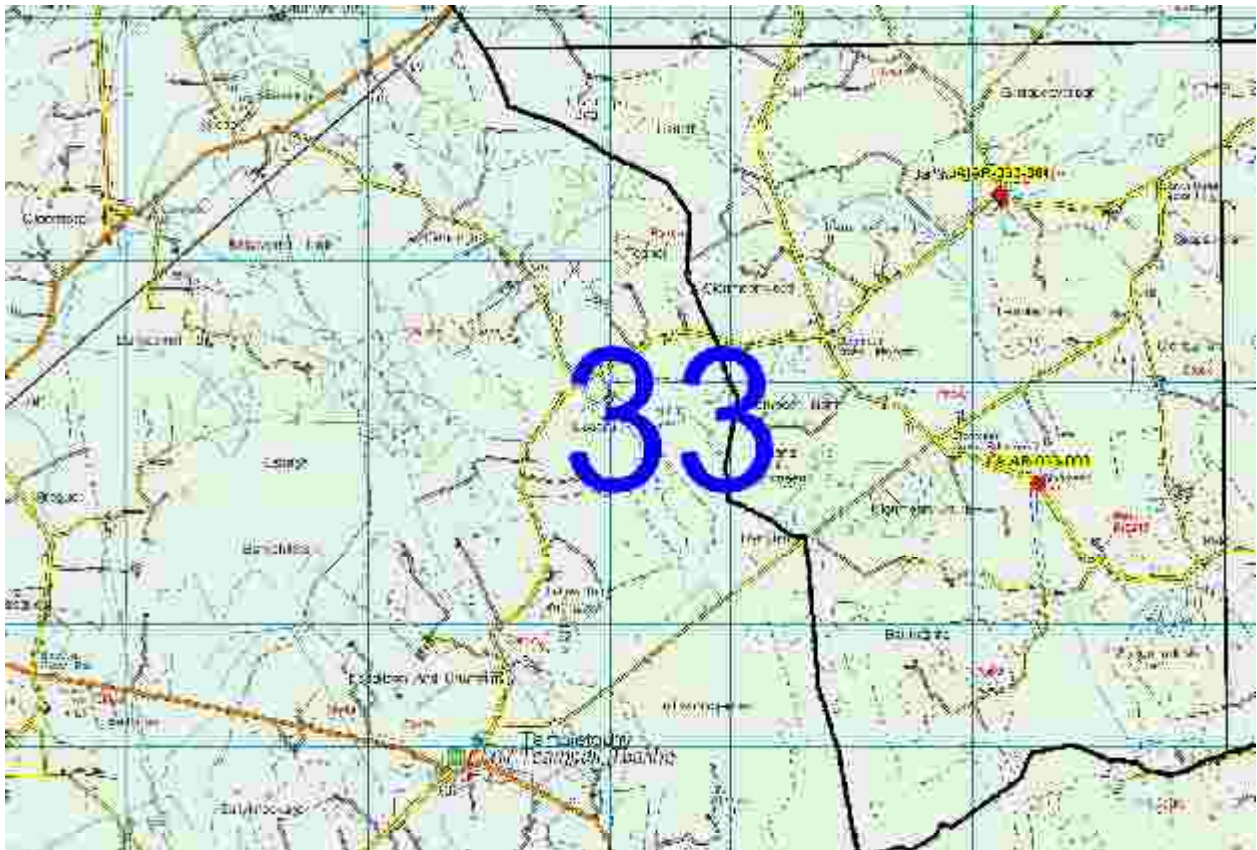
ABBEYLEIX

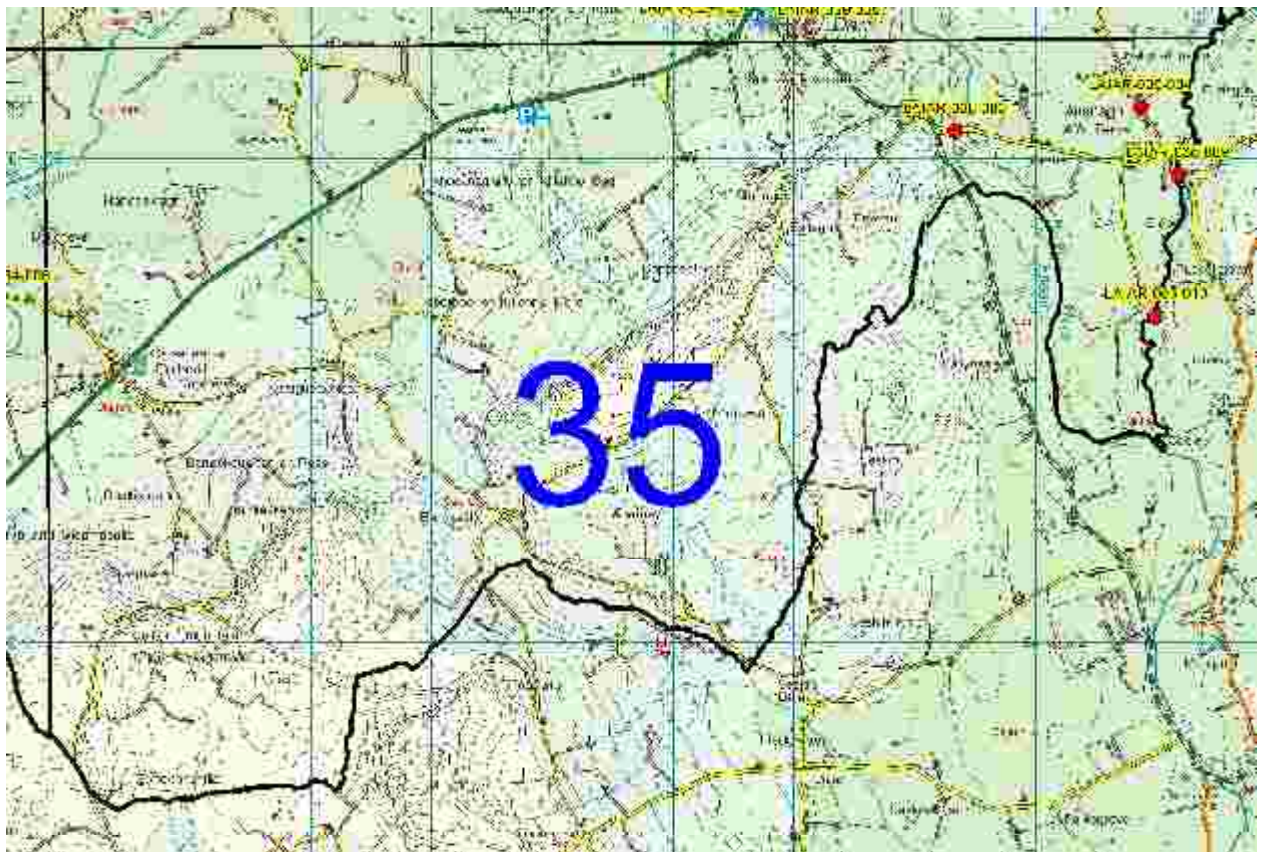


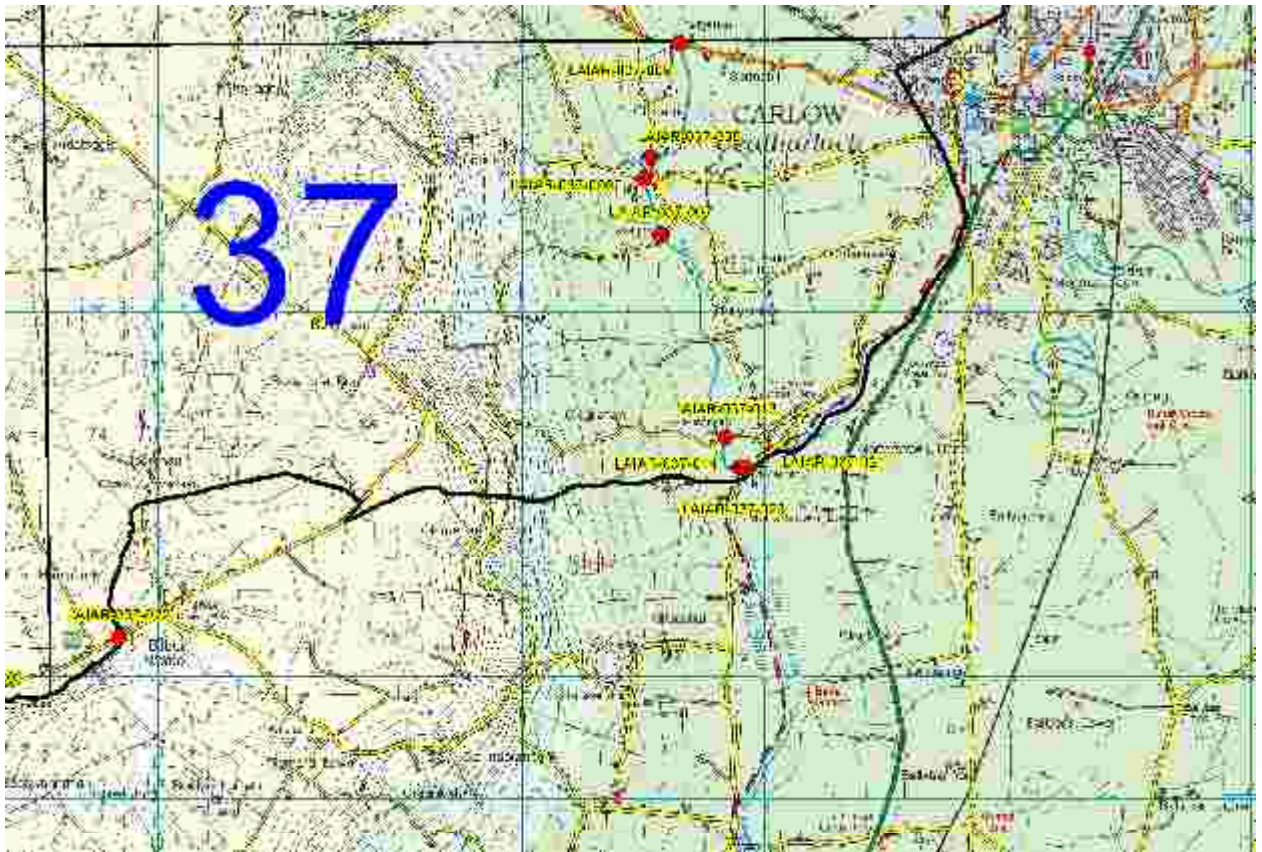
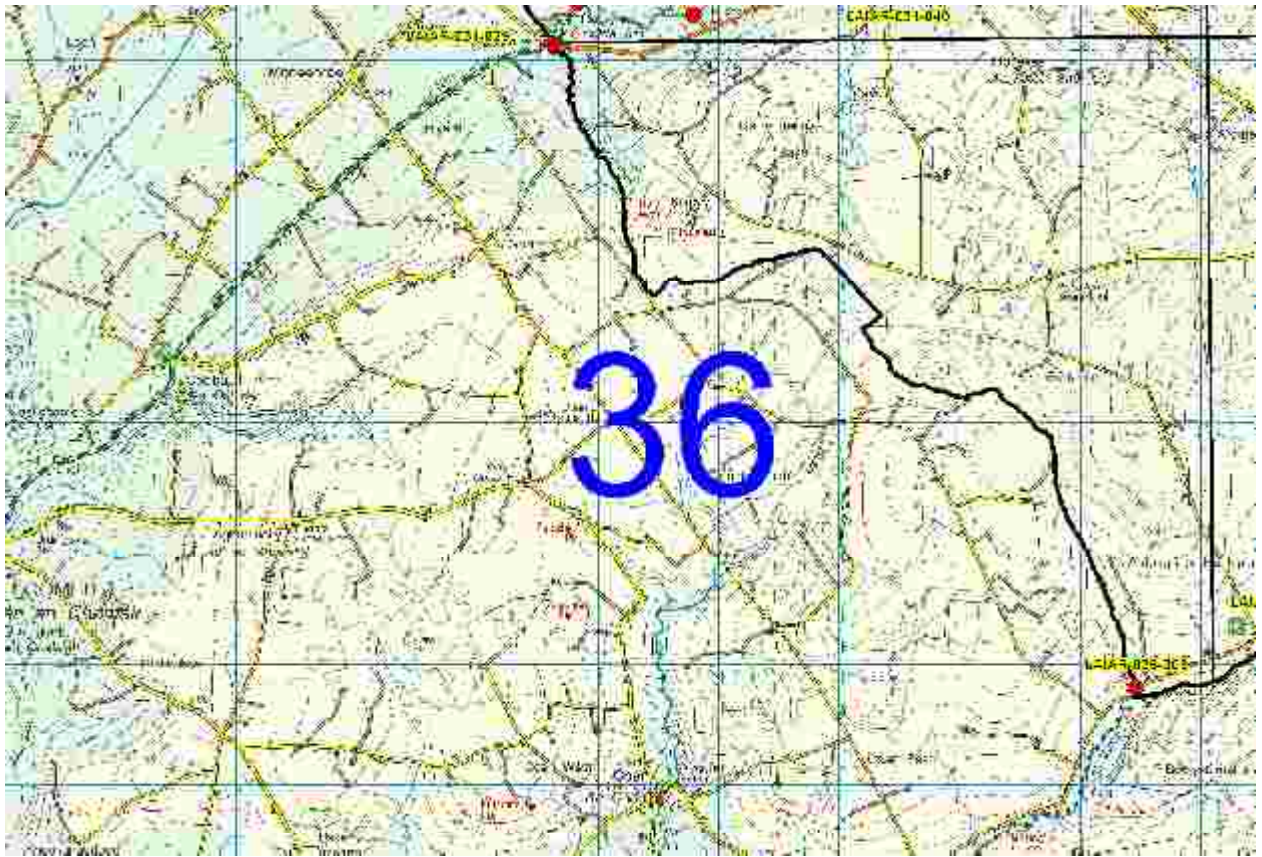












SITE INVENTORY

Ordered by LAIAR number

Site no LAIAR-002-005 **Gorragh Bridge**

County Laois **Townland** Ballyfarrell; Bunastick **Town** Clonaslee

Planning Laois CC **Discovery map** 54 **Six-inch map** 2

Summary An 18th/early 19th century twin-span arched masonry road bridge over Gorragh River. Widened with twin-span reinforced-concrete slab bridge in late 1900s.

History A bridge is shown on the 1839 and 1888 OS maps and explicitly captioned as Gorragh Bridge on the 1910 edition.

Component LAIAR-002-005 1

Type Bridge (road/river) Function Road & pedestrian

Category Transport Context Infrastructure

Irish Grid 232300 210761 +/- 10m

Survey date 10/04/2007 Surveyor Fred Hamond

Remains Complete Condition Good Current use Road bridge

Description A twin-span masonry arch and concrete beam bridge carries the Tullamore-Clonaslee road over the Gorragh River. The abutments, pier and cutwater on the upstream (south) side are of squared random rubble. The cutwater is of triangular profile and rises to arch crown level. The arches are of semicircular profile and have finely dressed voussoirs. The left- and right-bank arches are 4.81m and 4.60m wide respectively; their abutments are 5.71m deep. The spandrels are of coursed rubble. The parapets are of random rubble and are coped with squared blocks. The bridge has been widened by 7.64m on its downstream (north) side. The abutments, pier, triangular cutwater, span and parapet of this addition are all of concrete. The actual spans comprise reinforced-concrete slabs measuring 5.26m and 5.42m to the left and right banks respectively. A pipe is carried through the cutwater on the upstream side. The distance between the old and new parapets is 12.13m. There are footpaths on both sides of the road.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase LA Bridge R005

References

Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002). Page(s) 69

LAIAR-002-005_01 FWH 10/04/2007 Upstream (S) elevation.



LAIAR-002-005_02 FWH 10/04/2007 Downstream (N) elevation.



LAIAR-002-005_03 FWH 10/04/2007 Stone pitching on river bed (right-bank arch, looking upstream)



Site no LAIAR-002-006

County	Laois	Townland	Coolnamony Lower; Glebe (Tinnahinch By; Tinnahinch Ed.)	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	2

Summary An arched masonry accommodation bridge over Glenlahan River, giving access to 17th century castle. Possibly of late medieval date.

History A bridge is shown on the 1839, 1888 and 1910 OS maps. It appears to be associated with a nearby tower house and is possibly of 17th century date.

Component LAIAR-002-006 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	233861 209298	+/- 10m			
Survey date	10/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Fair	Current use	Road bridge
Description	An arched masonry bridge carries an accommodation track over the Glenlahan River, here running in a rocky gorge. It is of random rubble construction, save for its parapets which have been rebuilt in concrete. The arch originally had a semicircular profile, but is now flatter with settlement. It spans 3.91m and its abutments are 3.19m deep. Much of the mortar on the soffit has washed out. The distance between the parapets is 2.74m. The deck is curved.				

Interest Archaeological

Evaluation The juxtaposition of this bridge with a late medieval tower house makes it of archaeological interest. It is of local industrial archaeological significance and merits its current inclusion in the Record of Monuments & Places (LA002-01802-).

Rating Local **Protection** RMP LA002-01802- **Action** None

Associated with

Other dbase SMR LA002-01802-

References

Archaeological Survey of Ireland: Sites & Monuments Record print-out.	Page(s)
Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002).	Page(s) 114
Sweetman, P.D., Alcock, O. and Moran, B. Archaeological Inventory of County Laois (Dublin: Stationery Office, 1995).	Page(s) 128

LAIAR-002-006_01 FWH 10/04/2007 Upstream (S) elevation.



LAIAR-002-006_02 FWH 10/04/2007 Downstream (N) elevation.



Site no LAIAR-002-007

Lahoole Bridge

County Laois **Townland** Ballynahown; Garryhedder **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 2

Summary An 18th/early 19th century arched masonry accommodation bridge over a tributary of the Silver River.

History Lahoole Bridge is captioned on the 1839, 1888 and 1907 OS maps.

Component LAIAR-002-007 1

Type Bridge (road/river) **Function** Road & pedestrian
Category Transport **Context** Infrastructure
Irish Grid 225107 210139 +/- 10m
Survey date 10/04/2007 **Surveyor** Fred Hamond
Remains Substantial remains **Condition** Fair **Current use** Road bridge
Description An arched masonry bridge carries an accommodation track over a tributary of the Silver River. Of random rubble throughout. The arch is of semicircular profile and spans 2.45m; its abutments are 9.20m deep. The parapets are now missing apart from a vestige on the upstream side.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase

References

Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002). Page(s) 134

LAIAR-002-007_01 FWH 10/04/2007 Downstream (N) elevation.



Site no LAIAR-003-001

Wooden Bridge

County	Laois	Townland	Clonduff; Reary Beg	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	3

Summary Site of mid 19th century timber accommodation bridge over River Barrow. Replaced c.1930 with the present single-span concrete slab bridge by Board of Works in connection with Barrow Drainage Scheme.

History No road or bridge are shown on the 1839 OS map. A wooden bridge is cited on the 1888 and 1907-08 OS maps. The present concrete beam bridge probably dates from the Barrow Drainage Scheme of 1926-34.

Component LAIAR-003-001 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	237461 214673		+/- 10m		
Survey date	10/04/2007	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	Site of timber road bridge over River Barrow.				

Component LAIAR-003-001 2

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	237461 214674		+/- 10m		
Survey date	10/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A single-span concrete bridge carries an accommodation road over the River Barrow. It is of concrete throughout, with four-bar galvanised steel railings. The deck comprises a reinforced-concrete slab which spans 8.54m and is 3.75m wide.				

Interest Historical

Evaluation Although of some historical interest because of its association with the Barrow Drainage Scheme, this bridge is of no special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase

References

- Duffy, J. Barrow Bridges (Tullow: John Duffy, 2007). Page(s) 87
- Hogan, T.L. 'River Barrow Drainage', in Proc Inst Civil Engineers Ireland, vol.65 (1939). Page(s)
- Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002). Page(s) 106

LAIAR-003-001_01 FWH 10/04/2007 Downstream (N) elevation.



Site no	LAIAR-003-002	Ballyclare Bridge
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County	Laois	Townland	Clonduff; Drummond; Moneyquid	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	3

Summary Site of 18th/early 19th century road bridge over River Barrow. Destroyed by flood in 1990 and replaced by the present single-span concrete slab bridge in 1991.

History A bridge is shown here on Taylor & Skinner's 1783 road map. Ballyclare Bridge is captioned on the 1839, 1888 and 1907-08 OS maps. Destroyed by flood in 1990 and replaced by present bridge, just downstream, in 1991.

Component LAIAR-003-002 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	238536 214745	+/- 10m			
Survey date	10/04/2007	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	Site of arched masonry road bridge over River Barrow, destroyed by a flood in 1990 and replaced by the present road bridge just downstream (component 2).				

Component LAIAR-003-002 2

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	238559 214735	+/- 10m			
Survey date	10/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A single-span concrete bridge carries a minor road over River Barrow. It is of concrete construction throughout. The deck is a reinforced-concrete slab over pre-cast inverted T beams and spans 14.66m. The parapet comprises four-bar metal railings and crash barriers terminating in mass concrete piers set 10.23m apart. The latter are embellished with stonework set into the road and river faces. A metal plaque at the downstream left-bank end reads "Ballyclare Bridge/ opened February 1991/ replaced nearby masonry arch bridge/ destroyed by flood February 1990". The road was realigned slightly east of its original course to cross the new bridge. The river banks hereabouts are pitched with large stones.				

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase LA Bridge C008

References

Duffy, J. Barrow Bridges (Tullow: John Duffy, 2007). Page(s) 87
Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002). Page(s) 127

LAIAR-003-002_01 FWH 10/04/2007 Upstream (W) elevation.



LAIAR-003-002_02 FWH 10/04/2007 Plaque.



Site no **LAIAR-003-004** **Mucklone Bridge**

County Laois **Townland** Meelick (Tinnahinch By);
Mucklone **Town**

Planning Laois CC **Discovery map** 54 **Six-inch map** 3

Summary An 18th/early 19th century twin-arch masonry road bridge over River Barrow.

History A bridge is shown on the 1839, 1888 and 1907-08 OS maps. It is explicitly captioned as Mucklone Bridge on the 1907-08 map. Underpinned with concrete during Barrow Drainage Works of 1926-34.

Component **LAIAR-003-004 1**

Type Bridge (road/river) Function Road & pedestrian

Category Transport Context Infrastructure

Irish Grid 240492 213319 +/- 10m

Survey date 10/04/2007 Surveyor Fred Hamond

Remains Complete Condition Good Current use Road bridge

Description A twin-arch masonry bridge carries a minor road over the River Barrow. It is of random rubble throughout. There is a cutwater to the upstream (NW) side only, of semicircular profile and rising to just above arch spring level. The arches are of segmental profile and have finely dressed voussoirs. The left-bank arch spans 3.87m and the right-hand one 3.85m. The parapets, which are 5.80m apart, are coped with stones laid on their edges. The deck is curved. The abutments and piers are underpinned with concrete. John Duffy notes a bench mark and inscription reading "J.H. A.D. [?]" on it. The end of the downstream parapet is now missing.

Interest Architectural

Evaluation A modest example of a twin-arch masonry bridge over a significant river. Of local industrial heritage significance but not special enough to warrant inclusion in Record of Protected Structures.

Rating Local **Protection** **Action** None

Associated with

Other dbase LA Bridge C010

References

Duffy, J. Barrow Bridges (Tullow: John Duffy, 2007). Page(s) 88

Hogan, T.L. 'River Barrow Drainage', in Proc Inst Civil Engineers Ireland, Page(s)
vol.65 (1939).

LAIAR-003-004_01 FWH 10/04/2007 Upstream (NW) elevation. Note presence of cutwater.



LAIAR-003-004_02 FWH 10/04/2007 Downstream (SE) elevation. Note absence of cutwater.



Site no LAIAR-003-009

Arthur's Bridge

County Laois **Townland** Cappabeg; Cappaneary **Town**

Planning Laois CC **Discovery map** 54 **Six-inch map** 3

Summary Site of 18th/early 19th century road bridge over tributary of River Barrow. Replaced by the present single-span concrete beam and slab bridge in mid 1900s.

History A bridge is shown on the 1839, 1888 and 1907-08 OS maps. It is explicitly captioned as Arthur's Bridge on the 1907-08 map. The present bridge is a mid 20th century replacement. Now the responsibility of the National Roads Authority.

Component LAIAR-003-009 1

Type Bridge (road/river) Function Road & pedestrian

Category Transport Context Infrastructure

Irish Grid 241842 211840 +/- 10m

Survey date 10/04/2007 Surveyor Fred Hamond

Remains No visible remains Condition N/A Current use N/A

Description The original bridge which carried a minor road over a tributary of the River Barrow was replaced by the present bridge (component 2) in mid 20th century.

Component LAIAR-003-009 2

Type Bridge (road/river) Function Road & pedestrian

Category Transport Context Infrastructure

Irish Grid 241843 211840 +/- 10m

Survey date 10/04/2007 Surveyor Fred Hamond

Remains Complete Condition Good Current use Road bridge

Description A single-span concrete bridge carries a minor road over a tributary of the River Barrow. It is of concrete construction throughout. The deck comprises a reinforced-concrete slab over four concrete beams, all cast in-situ. It spans 3.58m and is 6.10m wide. The parapets are harled with cement.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase

References

Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002).

Page(s) 92

LAIAR-003-009_01 FWH 10/04/2007 Upstream (W) elevation.



Site no	LAIAR-003-010	Twomile Bridge
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County	Laois	Townland	Cappabeg; Forest Upper	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	3

Summary A slightly skew 18th/early 19th century triple-span road bridge over River Barrow. The middle of its original three stone arches replaced and all widened with concrete slab spans c.1930 by the Board of Works in connection with Barrow Drainage Scheme.

History A bridge is shown here on Moll's 1714 map. Twomile Bridge is captioned on the 1839, 1888 and 1907-08 OS maps. Its middle arch was replaced and the entire bridge widened during the Barrow Drainage Scheme of 1926-34. Now the responsibility of the National Roads Authority. So named because it is two Irish miles from Mountmellick (as is bridge LAIAR-008-050). Deck partly rebuilt and all widened in concrete during Barrow Drainage Works of 1926-34.

Component LAIAR-003-010 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	242251 211822 +/- 10m				
Survey date	10/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A slightly-skew triple-span masonry arch and concrete slab bridge carries the N80 Tullamore-Mountmellick road over the River Barrow. The upstream (NW) section is of random rubble construction. Its cutwater is of triangular profile and rises to arch crown level. There is a structural crack between the left-bank cutwater and its pier. The arches are of segmental profile; from the left to right banks, they measure 3.05m, 3.30m (estimated) and 3.26m (measured orthogonally). The voussoirs on the downstream side are of split rubble, whereas those to the upstream face are dressed. A break across all the soffits indicates that the bridge was widened; the higher quality stonework on the upstream voussoir suggests that the original bridge was extended on this side. The middle arch has been replaced with a reinforced-concrete slab, probably c.1930 in connection with the Barrow Drainage Scheme. The downstream section of the left-bank arch has been heavily repaired with shuttered concrete. The bridge has been widened by 4.95m on its downstream side, again probably in connection with the Barrow Drainage Scheme. The widened section comprises three reinforced-concrete slabs measuring 3.65m, 3.70m (estimated) and 3.13m (measured orthogonally from left to right bank). The new concrete downstream parapet is set 12.00m from the original upstream one. Concrete steps lead down to the left bank of the river just downstream.				

Interest Historical

Evaluation Although of some historical interest because of its association with the Barrow Drainage Scheme, this bridge is of no special industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with

References

Duffy, J. Barrow Bridges (Tullow: John Duffy, 2007). Page(s) 89

Hogan, T.L. 'River Barrow Drainage', in Proc Inst Civil Engineers Ireland, Page(s) vol.65 (1939).

Two Mile Bridge: Eirspan Inspection Report (National Roads Authority, 2002). Page(s)

LAIAR-003-010_01 FWH 10/04/2007 Upstream (NW) elevation.



LAIAR-003-010_02 FWH 10/04/2007 Downstream (SE) elevation.



LAIAR-003-010_03 FWH 10/04/2007 Soffit break in right-bank arch, looking upstream



Site no	LAIAR-003-011	Rathcoffey Bridge
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County	Laois	Townland	Rathcoffey	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	3

Summary 18th century road bridge over River Barrow. Replaced in mid 1800s by the present triple-arch masonry bridge.

History Rathcoffey Bridge is captioned on the 1839, 1888 and 1907-08 OS maps. It is noted as "in ruins" on the 1839 edition. Judging by its style, the present bridge is a mid 19th century replacement, very similar to Tinnahinch Bridge (LAIAR-003-014) just upstream.

Component LAIAR-003-011 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	234727 211068		+/- 10m		
Survey date	10/04/2007	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	Site of road bridge over River Barrow.				

Component LAIAR-003-011 2

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	234727 211068		+/- 10m		
Survey date	10/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A triple-arch masonry bridge carries a minor road over the River Barrow immediately downstream from its confluence with the Glenlahan River. It is of dressed and coursed masonry blocks throughout, except for its parapet which is of random rubble. The cutwaters are of triangular profile and rise to arch spring level. The arches are of segmental profile and have squared rubble masonry soffits. They measure from the left to right bank: 6.13m, 6.10m and 6.11m. Their voussoirs are finely dressed and are stepped into the spandrels. The dressed copings to the parapets have pitched profiles and are set 5.95m apart.				

Interest Architectural; Setting

Evaluation This is a substantial and well-constructed mid 19th century triple-arch bridge over a significant river. It is very similar to Tinnahinch Bridge, just upstream (LAIAR-003-014). It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.

Rating	Regional	Protection		Action	RPS
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Associated with

Other dbase LA Bridge C004

References

- Duffy, J. Barrow Bridges (Tullow: John Duffy, 2007). Page(s) 86
- Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002). Page(s) 179

LAIAR-003-011_01 FWH 10/04/2007 Upstream (E) elevation.



LAIAR-003-011_02 FWH 10/04/2007 Downstream (W) elevation.



LAIAR-003-011_03 FWH 10/04/2007 Stonework detail on pier etc.



Site no LAIAR-003-012

Clarahill Bridge

County	Laois	Townland	Clarahill; Rathcoffey	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	3

Summary An 18th/early 19th century arched masonry road bridge over River Barrow. Replaced by a metal bridge in the late 1800s (ironwork by Mountmellick Foundry, LAIAR-008-017). The present single-span reinforced-concrete bridge is a later 20th century replacement.

History Clarahill Bridge is captioned on the 1839, 1888 and 1907-08 OS maps. The original bridge was replaced in the late 1800s by a metal girder bridge manufactured by the Hibernian Ironworks, Mountmellick. The present bridge is a later 20th century replacement.

Component LAIAR-003-012 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	234562 210749		+/- 10m		
Survey date	10/04/2007	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	Site of arched masonry bridge carrying Mountmellick-Clonaslee Road over the Glenlahan River.				

Component LAIAR-003-012 2

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	234563 210749		+/- 10m		
Survey date	10/04/2007	Surveyor	Fred Hamond		
Remains	Some remains	Condition	Fair	Current use	Disused
Description	A single-span metal beam bridge formerly carried the Mountmellick-Clonaslee Road over the Glenlahan River. Only the squared and coursed rubble masonry abutments now survive, partly incorporated in the present bridge.				

Component LAIAR-003-012 3

Type	Bridge (road/river)	Function	Road & pedestrian
Category	Transport	Context	Infrastructure
Irish Grid	234565 210756 +/- 10m		
Survey date	10/04/2007	Surveyor	Fred Hamond
Remains	Complete	Condition	Good
		Current use	Road bridge
Description	A single-span concrete bridge carries the Mountmellick-Clonaslee Road over the Glenlahan River. The present road is realigned slightly downstream from its previous course. The abutments and span are of reinforced-concrete. The latter is 9.00m wide and comprises pre-cast reinforced-concrete beams and slab deck. The parapets comprise four-bar galvanised steel railings set 13.38m apart. They terminate in coursed rubble masonry piers coped with concrete. A pipe is carried across the upstream side, level with the deck.		

Interest None**Evaluation** No special industrial heritage significance.**Rating** Record only **Protection** **Action** None**Associated with****Other dbase** LA Bridge R006**References**

- Culliton, T. Legends and Folklore of Mountmellick (Mountmellick: Mountmellick Chamber of Commerce, 1977). Page(s) 10
- Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002). Page(s) 101

LAIAR-003-012_01 FWH 10/04/2007 Downstream (N) elevation.



LAIAR-003-012_02 FWH 10/04/2007 Girder bridge abutments on upstream left bank.



Site no	LAIAR-003-014	Tinnahinch Bridge
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County	Laois	Townland	Drumnabehy; Tinnahinch	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	3

Summary 18th/early 19th century timber road bridge over River Barrow. Replaced by the present triple-arch masonry bridge in mid 19th century.

History A bridge is shown here on Moll's 1714 map. Tinnahinch Bridge is captioned on the 1839, 1888 and 1907-08 OS maps. It is noted as a wooden bridge on the 1839 edition and was presumably replaced by the present masonry bridge in the mid 1800s. It is similar to Rathcoffey Bridge, just downstream (LAIAR-003-011).

Component LAIAR-003-014 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	235167 210486		+/- 10m		
Survey date	10/04/2007	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	Site of wooden bridge carrying road over River Barrow.				

Component LAIAR-003-014 2

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	235167 210486		+/- 10m		
Survey date	10/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A triple-arch masonry bridge carries the Mountmellick-Clonaslee road over the River Barrow. It is of dressed masonry blocks brought to courses, except for its parapet which is of random rubble. The cutwaters are of curved profile and rise to arch spring level. The voussoirs are finely dressed and are stepped into the spandrels. The arches are of segmental profile and have rubble masonry soffits. They measure from the left to right bank: 6.06m, 6.08m and 6.07m. The parapets are probably relatively recent rebuilds and are set 6.51m apart; their original stone copings have been replaced with concrete. The upstream cutwater towards the left bank has also been replaced in mass concrete. The deck is curved and has dogleg approaches.				

Interest Architectural; Historical

Evaluation This is a substantial and well-constructed mid 19th century triple-arch bridge over a significant river. It is very similar to Rathcoffey Bridge, just downstream (LAIAR-003-011). It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.

Associated with

Other dbase LA Bridge R007

References

Duffy, J. Barrow Bridges (Tullow: John Duffy, 2007). Page(s) 85
Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002). Page(s) 197

LAIAR-003-014_01 FWH 10/04/2007 Upstream (S) elevation.



LAIAR-003-014_02 FWH 10/04/2007 Downstream (N) elevation.



LAIAR-003-014_03 FWH 10/04/2007 Stonework detail on downstream face.



LAIAR-003-014_04 FWH 10/04/2007 Falsework socket on left-bank abutment.



Site no LAIAR-003-024

Wooden Bridge

County	Laois	Townland	Clonduff; Reary Beg	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	3

Summary Site of 18th/early 19th century timber accommodation bridge over River Barrow.

History A wooden bridge is cited on the 1839 OS map. No bridge or crossing are shown on the 1888 or 1907-08 maps. It was presumably removed in the mid 1800s, possibly as a consequence of river dredging.

Component LAIAR-003-024 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	237319 214104		+/- 10m		
Survey date	10/04/2007	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	No traces survive of timber accommodation bridge over River Barrow.				

Interest None

Evaluation No special industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with

Other dbase

References

Site no LAIAR-003-026

County	Laois	Townland	Forest Upper; Graigue (Tinnahinch By; Graigue Ed.)	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	3

Summary An 18th/early 19th century triple-arch masonry road bridge over River Barrow.

History A bridge is shown on the 1839, 1888 and 1907-08 OS maps.

Component LAIAR-003-026 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	243039 210137		+/- 10m		
Survey date	10/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge

Description A triple-arch masonry bridge carries a minor road over the River Barrow. It is of random rubble construction throughout. The arches are of segmental profile. There are cutwaters on the upstream (NW) side only; they are of squared random rubble, of semicircular profile, and rise to arch spring level. The arches span, from left to right bank: 3.69m, 3.75m (estimated) and 3.90m (estimated). The parapets are coped with stones laid on edge and are set 3.82m apart.

Interest Architectural

Evaluation A good example of a triple-arch masonry bridge, albeit of modest scale, across a significant river. Of local industrial heritage significance but not sufficiently special to warrant inclusion in Record of Protected Structures.

Rating	Local	Protection		Action	None
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Associated with

Other dbase

References

Duffy, J. Barrow Bridges (Tullow: John Duffy, 2007).

Page(s) 89

LAIAR-003-026_01 FWH 10/04/2007 Upstream (NW) elevation.



Site no LAIAR-003-027

Wooden Bridge

County	Laois	Townland	Forest Upper; Graigue (Tinnahinch By; Graigue Ed.)	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	3

Summary Site of 18th/early 19th century timber road bridge over River Barrow, giving access to Forrest House. The present twin-span metal beam and concrete slab bridge is a c.1930 replacement by Board of Works in connection with Barrow Drainage Scheme.

History A bridge is shown on the 1839, 1888 and 1907-08 OS maps. It is explicitly captioned as a wooden bridge on the 1839 and 1888 maps. The present bridge dates from the Barrow Drainage Scheme of 1926-34.

Component LAIAR-003-027 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	243501 209775	+/- 10m			
Survey date	10/04/2007	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	No traces survive of this wooden bridge which formerly carried an accommodation track over the River Barrow.				

Component LAIAR-003-027 2

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	243501 209775	+/- 10m			
Survey date	10/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Fair	Current use	Road bridge
Description	A twin-span metal beam and concrete bridge carries an accommodation track over the River Barrow. The abutments, piers and cutwaters are of concrete. The latter are of semicircular profile on both the up- and downstream sides. The deck comprises a 3.35m wide reinforced-concrete slab over four RSJs (of 31cm x 13cm section). Both spans are 6.61m across. The parapets comprise three-bar tubular steel railings.				

Interest Historical

Evaluation Although of some historical interest because of its association with the Barrow Drainage Scheme, this bridge is of no special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase

References

Duffy, J. Barrow Bridges (Tullow: John Duffy, 2007).

Page(s) 90

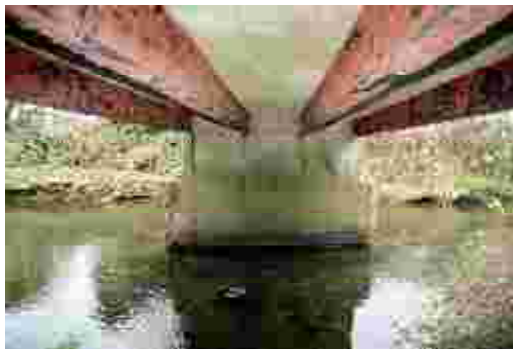
Hogan, T.L. 'River Barrow Drainage', in Proc Inst Civil Engineers Ireland, vol.65 (1939).

Page(s)

LAIAR-003-027_01 FWH 10/04/2007 Upstream (NW) elevation.



LAIAR-003-027_02 FWH 10/04/2007 Underside of deck, from left bank.



Site no LAIAR-003-028

County Laois **Townland** Tinnahinch **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 3

Summary Mid 19th century arched masonry road bridge over River Barrow.

History No road or bridge are on the 1839 OS map. An uncaptioned bridge is shown on the 1888 and 1907-08 OS maps. It is therefore of mid 19th century date.

Component LAIAR-003-028 1

Type Bridge (road/river) **Function** Road & pedestrian
Category Transport **Context** Infrastructure
Irish Grid 235563 209686 +/- 10m
Survey date 10/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge

Description An arched masonry bridge carries a minor road over the River Barrow. It is of random rubble throughout, save for the upstream (SE) parapet which has been rebuilt in concrete. The arch springs off the bedrock and is of segmental profile; it measures 9.21m wide. The parapets are relatively narrow at only 3.09m apart. The edge-set stone coping to the downstream parapet, part of which has been rebuilt, is relatively recent.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase LA Bridge C002

References

Duffy, J. Barrow Bridges (Tullow: John Duffy, 2007).

Page(s) 84

LAIAR-003-028_01 FWH 10/04/2007 Downstream (NW) elevation.



LAIAR-003-028_02 FWH 10/04/2007 Voussoir detail on downstream left-bank arch.



LAIAR-003-028_03 FWH 10/04/2007 Rebuilt downstream parapet. Note copings.



Site no LAIAR-003-043

County	Laois	Townland	Cappaneary; Dernacart	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	3

Summary Site of late 18th/early 19th century accommodation bridge over River Barrow. Replaced in 1900s by the present skew twin-span concrete slab bridge, probably c.1930 by Board of Works in connection with Barrow Drainage Scheme.

History A bridge is shown on the 1839, 1888 and 1907-08 OS maps. The present bridge is a 20th century replacement and is probably associated with the Barrow Drainage Scheme of 1926-34.

Component LAIAR-003-043 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	242003 212504		+/- 10m		
Survey date	10/04/2007	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	Site of bridge carrying accommodation road over River Barrow.				

Component LAIAR-003-043 2

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	242004 212500		+/- 10m		
Survey date	10/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A twin-span concrete bridge carries an accommodation track over the River Barrow. It is of mass concrete, with two-bar metal railings. The 3.66m wide reinforced-concrete slab deck spans 4.53m and 4.59m on its left- and right-banks respectively.				

Interest None

Evaluation Although of some historical interest because of its association with the Barrow Drainage Scheme, this bridge is of no special industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with

Other dbase

References

Hogan, T.L. 'River Barrow Drainage', in Proc Inst Civil Engineers Ireland, vol.65 (1939).

Page(s)

LAIAR-003-043_01 FWH 10/04/2007 Downstream (S) elevation.



Site no LAIAR-003-044

County Laois **Townland** Cappaneary; Dernacart **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 3

Summary Modern timber foot bridge over River Barrow.

History This bridge dates from the 1990s or early 2000s.

Component LAIAR-003-044 1

Type Bridge (foot/river) **Function** Road & pedestrian
Category Transport **Context** Infrastructure
Irish Grid 237315 214075 +/- 10m
Survey date 10/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Foot bridge
Description A single-span timber foot bridge over the River Barrow. It comprises two parallel ESB telegraph poles set into the bank and spanning c.13m. Timber slats nailed to their tops form the rudimentary deck. Chicken wire mesh handrail on upstream (south) side.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase

References

LAIAR-003-044_01 FWH 10/04/2007 Downstream (N) elevation.



LAIAR-003-044_02 FWH 10/04/2007 Looking west across deck.



Site no LAIAR-004-001

Bay Bridge

County Laois **Townland** Forest Lower; Townparks
(Tinnahinch By) **Town**

Planning Laois CC **Discovery map** 54 **Six-inch map** 4

Summary Mid 19th century twin-span masonry arch road bridge over River Barrow, on site of 18th century ford.

History A bridge is first cited here on the 1888 OS map. It augmented a ford to its west. As it is not on the 1839 map, it is therefore of mid 19th century date. It is captioned as Bay Bridge on the 1907 map.

Component LAIAR-004-001 1

Type Bridge (road/river) **Function** Road & pedestrian

Category Transport **Context** Infrastructure

Irish Grid 245459 209210 +/- 10m

Survey date 11/04/2007 **Surveyor** Fred Hamond

Remains Complete **Condition** Good **Current use** Road bridge

Description A twin-span masonry arch road bridge carries a public road over the River Barrow. The abutments and piers are of rock-faced masonry blocks laid to courses. The abutments and downstream ends of the piers have tooled quoins. There are finely dressed curved masonry cutwaters to the upstream (west) ends of the piers, rising to arch spring level. The arches are of shallow segmental profile and have rusticated voussoirs with margined edges. The left-bank arch spans 7.26m and the right-hand one 7.31m. The arch soffits are of rubble masonry. The spandrels and parapets are also of squared masonry, but randomly laid. A string course runs along the base of the parapets. The parapets, which are 4.95m apart, are coped with dressed masonry blocks and have rounded ends. The bed of the river has been concreted.

Interest Architectural

Evaluation This bridge is of architectural interest because of its high quality of construction and detailing of its stonework. It is a good example of a mid 19th century road bridge over a significant river. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.

Rating Regional **Protection** **Action** RPS

Associated with

Other dbase LA Bridge C024

References

Duffy, J. Barrow Bridges (Tullow: John Duffy, 2007). Page(s) 90

Scott, M. Mountmellick Pictorial Memories: Book 2 (1998). Page(s) 9

LAIAR-004-001_01 FWH 11/04/2007 Upstream (W) elevation.



LAIAR-004-001_02 FWH 11/04/2007 Downstream (E) elevation.



LAIAR-004-001_03 FWH 11/04/2007 Upstream pier detail.



LAIAR-004-001_04 FWH 11/04/2007 Downstream pier detail. Note absence of cutwater.



LAIAR-004-001_05 FWH 11/04/2007 Parapet end detail.



Site no LAIAR-004-002 **Cottoner's Bridge**

County Laois; Offaly **Townland** Forest Lower; Co Offaly **Town**
Planning Laois CC; Offaly CC **Discovery map** 54 **Six-inch map** 4

Summary A masonry arch road bridge of 18th//early 19th century date over Cottoner's Brook at county boundary.

History Cottoner's Bridge is cited on the 1839, 1888 and 1907 OS maps.

Component LAIAR-004-002 1

Type Bridge (road/river) **Function** Road & pedestrian
Category Transport **Context** Infrastructure
Irish Grid 246626 209644 +/- 10m
Survey date 11/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge

Description An arched masonry bridge carries a public road over Cottoner's Brook on the boundary with Co Offaly. Of random rubble construction throughout. Abutments faced with cement render. The arch, which spans 3.05m, has split stone voussoirs. The parapets are spaced at 8.30m and coped with concrete (abutments are 9.40m long). Bed and banks are pitched with concrete. Heavily overgrown with ivy.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase LA Bridge R013; OFIAR-033-007

References

LAIAR-004-002_01 FWH 17/06/2004 West (upstream) face.



Site no LAIAR-004-003 **Borness Bridge; {Barranagh's Bridge}**

County	Laois	Townland	Forest Lower; Strahard	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	4

Summary Site of 18th/early 19th century masonry arch road bridge over River Barrow. Replaced c.1930 with a twin-span metal beam and concrete slab bridge by Board of Works in connection with Barrow Drainage Scheme.

History Barranagh's Bridge is cited on the 1839 and 1888 OS maps, and Borness Bridge on the 1907 map. Originally a masonry arch bridge, it was replaced by the present metal/concrete bridge during the Barrow Drainage Scheme of 1926-34.

Component LAIAR-004-003 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	246355 209284		+/- 10m		
Survey date	11/04/2007	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	Site of late 18th/early 19th century road bridge over River Barrow.				

Component LAIAR-004-003 2

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	246355 209284		+/- 10m		
Survey date	11/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A twin-span reinforced-concrete bridge of later 20th century date carries a public road over the River Barrow. The abutments and piers are of concrete. There are rounded cutwaters to the up and downstream piers. The reinforced-concrete deck rests on six 45cm x 15cm RSJs. Both spans measure 6.09m. The parapets are of concrete and are spaced at 6.27m. The bed of the river has been concreted.				

Interest Architectural; Historical

Evaluation This bridge is a good example of its type (metal beam and concrete slab) and of some historical interest because of its association with the Barrow Drainage Scheme. It is of local industrial heritage significance.

Rating	Local	Protection		Action	None
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Associated with

References

Duffy, J. Barrow Bridges (Tullow: John Duffy, 2007). Page(s) 91

Hogan, T.L. 'River Barrow Drainage', in Proc Inst Civil Engineers Ireland, vol.65 (1939). Page(s)

Scott, M. Mountmellick Pictorial Memories: Book 2 (1998). Page(s) 13

LAIAR-004-003_01 FWH 11/04/2007 Upstream (W) elevation. Note flow gauge at right.



LAIAR-004-003_02 FWH 11/04/2007 Downstream (E) elevation.



LAIAR-004-003_03 FWH 11/04/2007 Detail of deck underside showing steel beams.



Site no	LAIAR-004-004	Portnahinch Bridge
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County	Laois; Offaly	Townland	Portnahinch; Co Offaly	Town	
Planning	Laois CC; Offaly CC	Discovery map	54	Six-inch map	4

Summary A triple-span masonry arch road bridge of 18th/early 19th century date over River Barrow.

History A bridge is shown on Moll's 1714 map. Also shown on the 1839, 1888 and 1907 OS maps. Explicitly captioned at Portnahinch Bridge on the 1907 edition. John Duffy dates the present bridge to about 1795.

Component LAIAR-004-004 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	249062 210046 +/- 10m				
Survey date	11/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A triple-arch masonry bridge carries a public road over the River Barrow on the boundary with Co Offaly. The abutments and piers are of regularly coursed dressed masonry blocks. The piers have rounded cutwaters at both ends, rising to arch spring level and coped with concrete. Over the upstream cutwaters are decorative semicircular-headed blind niches. The arches are of segmental profile and have finely dressed voussoirs with projecting keystones. From the left (Offaly) to right (Laois) bank, the spans are span 7.60m, 9.15m and 7.60m. A string course runs through each arch at spring level. The spandrels and parapets are of random rubble; the latter are 6.50m apart and are coped with roughly dressed stones. The west end of the downstream parapet has been damaged and there is extensive ivy overgrown on the upstream side. The approach roads are doglegged and the deck has a slight curve. The abutments and piers are underpinned with concrete.				

Interest Architectural; Setting

Evaluation Architecturally, this bridge is of interest in terms of its scale, modest embellishments and style (string course, blind niches), all typical of an 18th or early 19th century Grand Jury construction. It also enhances the riverscape. It is of regional heritage significance and merits inclusion in the Record of Protected Structures.

Rating	Regional	Protection	Action	RPS
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Associated with

Other dbase LA Bridge C017; OFIAR-033-005

References

Duffy, J. Barrow Bridges (Tullow: John Duffy, 2007).

Page(s) 92-93

LAIAR-004-004_01 FWH 17/06/2004 Upstream (N) elevation.



LAIAR-004-004_02 FWH 17/06/2004 Downstream (S) elevation.



LAIAR-004-004_03 FWH 17/06/2004 Downstream (S) face.



LAIAR-004-004_04 FWH 11/04/2007 Upstream (N) pier and spandrel detail.



Site no	LAIAR-004-006	Kilnahown Bridge
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County	Laois; Offaly	Townland	Coolnavarnoge and Coolaghy; Co Offaly	Town	
Planning	Laois CC; Offaly CC	Discovery map	54	Six-inch map	4

Summary A four-arch masonry road bridge of later 18th/early 19th century date over River Barrow at boundary with Co Offaly. Also two flood arches on Co Laois side.

History Kilnahown Bridge is cited on the 1839, 1888 and 1907 OS maps. It dates from the later 1700s or early 1800s.

Component LAIAR-004-006 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	251350 210693 +/- 10m				
Survey date	11/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge

Description A four-arch masonry bridge carries a public road over the River Barrow at the boundary with Co Offaly. Of random rubble construction throughout. Originally there were angled cutwaters on the upstream (SW) ends of the piers only, rising to just above arch spring level. Mass concrete cutwaters were added on the downstream side and rise to spring level only. The arches, which rise to the centre of the bridge, are of semicircular profile and have dressed stone voussoirs. From the left (Offaly) to right (Laois) bank, their spans are: 6.08m, 4.78m, 5.41m and 4.82m. The parapets are 6.54m apart and are coped with roughly dressed masonry blocks. The deck has a slight curvature. The bases of the abutments and piers are underpinned with concrete. There are also two small flood arches, identical to the main arches, on the Laois side. They are silted up, the end one to such an extent that it is not visible on its SW side. The voussoir at the NE end of this arch has collapsed and the approach walls on the Laois side lean outwards.

Interest Architectural; Setting

Evaluation Architecturally, this bridge is of interest in terms of its scale, unadorned style typical of an 18th/early 19th century Grand Jury construction. It also enhances the riverscape hereabouts. It is of regional heritage significance and merits inclusion in the Record of Protected Structures.

Rating	Regional	Protection	Action	RPS
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Associated with

Other dbase LA Bridge C016; Mountmellick Canal F928; OFIAR-033-006

References

Duffy, J. Barrow Bridges (Tullow: John Duffy, 2007).

Page(s) 94-95

LAIAR-004-006_01 FWH 17/06/2004 Upstream (SW) elevation. Note flood arch on bank at right.



LAIAR-004-006_02 FWH 17/06/2004 Upstream (SW) elevation, from left (Offaly) bank.



LAIAR-004-006_03 FWH 11/04/2007 Upstream (SW) elevation from right (Laois) bank.



LAIAR-004-006_04 FWH 17/06/2004 Downstream (NE) elevation, from right (Laois) bank.



LAIAR-004-006_05 FWH 11/04/2007 Flood arch nearest river on Laois (right bank) side.



LAIAR-004-006_06 FWH 11/04/2007 Collapsed flood arch at NE side of end flood arch on Laois side.



Site no LAIAR-004-007

Tinnakill Lock

County Laois **Townland** Coolnavarnoge and Coolaghy **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 4

Summary Site of foot bridge over Mountmellick Branch of Grand Canal at Tinnakill Lock. Also the remains of a lock and lock keeper's house, all of c.1828, at this site.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. This bridge, at Tinnakill Lock, is shown on the 1839, 1888 and 1907 OS maps.

Component LAIAR-004-007 3

Type Bridge (foot/canal) **Function** Inland waterway
Category Transport **Context** Infrastructure
Irish Grid 250586 209520 +/- 10m
Survey date 29/06/2006 **Surveyor** Fred Hamond
Remains No visible remains **Condition** N/A **Current use** N/A
Description All traces of this foot bridge, at the NE end of Tinnakill lock chamber, have been removed.

Interest None

Evaluation No industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with LAIAR-004-005

Other dbase Mountmellick Canal F301b

References

Site no LAIAR-004-008

County Laois **Townland** Coolnavarnoge and Coolaghy **Town**

Planning Laois CC **Discovery map** 54 **Six-inch map** 4

Summary A culvert of c.1828 carries a minor tributary of the River Barrow under the Mountmellick Branch of the Grand Canal.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. This aqueduct is explicitly captioned on the 1839, 1888 and 1907 OS maps.

Component LAIAR-004-008 1

Type Bridge (canal/river) Function Inland waterway

Category Transport Context Infrastructure

Irish Grid 251073 209920 +/- 10m

Survey date 29/06/2006 Surveyor Fred Hamond

Remains Complete Condition Good Current use Culvert

Description An arched masonry culvert carries a minor tributary of the River Barrow under the bed of the now-filled Mountmellick Canal. Constructed in squared limestone rubble brought to courses. The arch is of segmental profile and has dressed voussoirs; 90cm span. Heavily vegetated at SE end.

Interest Historical

Evaluation Still serves original purpose and marks line of now-gone section of canal. Local industrial heritage significance.

Rating Local **Protection** **Action** None

Associated with LAIAR-004-005

Other dbase Mountmellick Canal F209

References

LAIAR-004-008_01 FWH 29/06/2006 General view of culvert from north-west.



LAIAR-004-008_02 FWH 29/06/2006 North-elevation of culvert.



Site no **LAIAR-004-009** **Woodbrook Bridge**

County Laois **Townland** Coolnavarnoge and Coolaghy **Town**

Planning Laois CC **Discovery map** 54 **Six-inch map** 4

Summary Site of road bridge of c.1828 over Mountmellick Branch of Grand Canal.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. This bridge is shown on the 1839 and 1888 OS maps and cited as Woodbrook Bridge on the 1907 edition.

Component **LAIAR-004-009 1**

Type Bridge (road/canal) Function Inland waterway

Category Transport Context Infrastructure

Irish Grid 251543 210263 +/- 10m

Survey date 29/06/2006 Surveyor Fred Hamond

Remains No visible remains Condition N/A Current use N/A

Description The bridge carrying a public road over the Mountmellick Canal has been demolished and a wider road constructed across this now-infilled section of canal.

Interest None

Evaluation No industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with LAIAR-004-005

Other dbase IWAI MMC 140; Mountmellick Canal F207

References

Orford, D. Those Were the Days: Memories of Portarlinton, 1900 to 1999 Page(s) 244
(Portarlinton: Portarlinton Historical Society).

Site no LAIAR-004-010 **Blackhall Bridge; {Moore's Bridge}**

County	Laois	Townland	Ballymorris	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	4

Summary Masonry arch road bridge of c.1828 over Mountmellick Branch of Grand Canal. Now by-passed by main road.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. Moore's Bridge is cited on the 1839 and 1888 OS maps, and Blackhall Bridge on the 1907 edition. Attempts to demolish the bridge after the canal closed in 1960 were abandoned and what remained was restored by a local community committee as a millennium project in 2000.

Component LAIAR-004-010 1

Type	Bridge (road/canal)	Function	Inland waterway		
Category	Transport	Context	Infrastructure		
Irish Grid	253128 210795		+/- 10m		
Survey date	29/06/2006	Surveyor	Fred Hamond		
Remains	Substantial remains	Condition	Good	Current use	Disused
Description	This masonry arch bridge formerly carried a public road over a now-infilled section of Mountmellick Branch of Grand Canal. Now by-passed by a new section of road to its south-west. Although the bed of the canal under the bridge has been infilled to within 30cm of the tops of the side walls, the bridge was restored in 2000 and is now a distinctive landmark on the way into Portarlinton from the south. Of randomly coursed squared limestone rubble. The arch is slightly skew and of semicircular profile, with finely dressed voussoirs. It spans 6.69m (22ft) measured orthogonally, and 7.00m (23ft) on the skew. Although skew, the squared soffit blocks are laid orthogonally. A 1.71m wide (5ft 7in) towpath runs through its south-east side; the channel is 4.98m wide (16ft 4in) at this point; its edge is coped with large blocks of limestone. Both spandrels and the curved wing walls have been largely rebuilt in squared limestone rubble (of slightly darker colour than the original). The tops of the out-stepped terminal piers have been removed along with the decorative string course which originally ran under the parapets; much of the north wing and pier have been removed entirely. The parapets have been rebuilt in concrete blockwork, faced with stone, and coped with concrete; they are 6.16m (20ft 3in) apart. Humped deck. No datestones. The approach road has been removed at the north end.				

Interest Architectural; Historical; Setting

Evaluation The architectural interest of this bridge is diminished somewhat by the introduced stone (necessitated by its partial demolition in 1970) and use of concrete blockwork in its refurbishment. Nevertheless it is still of historical interest and a well-known landmark hereabouts. It is of regional industrial heritage significance and its current inclusion in the Record of Protected Structures is justified (LA 542).

Rating	Regional	Protection	RPS LA 542	Action	None (in RPS)
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Associated with LAIAR-004-005

References

Orford, D. Those Were the Days: Memories of Portarlinton, 1900 to 1999 Page(s) 76
(Portarlinton: Portarlinton Historical Society).

Orford, D. Those Were the Days: Memories of Portarlinton, 1900 to 1999 Page(s) 249
(Portarlinton: Portarlinton Historical Society).

Somers, P. 'Major initiative planned for former Portarlinton canal bridge'. In Page(s) 33
The Leinster Express, 3 July 1999.

Survey of the Architectural Heritage of County Laois (Dublin: NIAH, 2002). Page(s)

LAIAR-004-010_01 FWH 29/06/2006 Plaque commemorating restoration of bridge.



LAIAR-004-010_02 FWH 29/06/2006 Blackhall Bridge from south-west.



LAIAR-004-010_03 FWH 29/06/2006 Blackhall Bridge from north.



LAIAR-004-010_04 FWH 29/06/2006 Blackhall Bridge from east.



LAIAR-004-010_05 FWH 29/06/2006 Soffit detail.



LAIAR-004-010_06 FWH 29/06/2006 Tow rope marks on east quoin.



Site no LAIAR-004-011

County	Laois	Townland	Ballymorris	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	4

Summary A culvert of c.1828 carries a tributary of the River Barrow under the Mountmellick Branch of the Grand Canal.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. This aqueduct is cited on the 1839, 1888 and 1907 OS maps.

Component LAIAR-004-011 1

Type	Bridge (canal/river)	Function	Inland waterway		
Category	Transport	Context	Infrastructure		
Irish Grid	253150 210818		+/- 10m		
Survey date	29/06/2006	Surveyor	Fred Hamond		
Remains	Complete	Condition	Poor	Current use	Culvert
Description	A twin-arched culvert carries a stream under the now-demolished bed of the Mountmellick Canal. Of squared random limestone construction. The arches are of segment profile with dressed voussoirs; each spans 90cm. There is no cutwater. Most of south-east face has collapsed. Both channels are now heavily choked with silt and overgrown.				

Interest Historical

Evaluation Primarily of historical interest due to canal association. Local industrial heritage significance.

Rating	Local	Protection		Action	None
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Associated with LAIAR-004-005

Other dbase Mountmellick Canal F205

References

LAIAR-004-011_01 FWH 29/06/2006 Decayed south-east face of culvert.



Site no **LAIAR-004-014** **Deerpark Bridge**

County Laois **Townland** Ballymorris **Town**
Planning Laois CC **Discovery map** 55 **Six-inch map** 4

Summary Skew arched masonry road bridge over Dublin-Cork railway line, opened by Great Southern & Western Railway Co in 1847. Soffit is of skewly-laid brickwork.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway was opened in 1847. An uncaptioned bridge is marked on the 1888 OS map and Deerpark Bridge on the 1907 edition. It was widened in the later 1900s.

Component LAIAR-004-014 1

Type Bridge (road/rail) **Function** Railway
Category Transport **Context** Infrastructure
Irish Grid 253887 210494 +/- 10m
Survey date 11/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge

Description A skewed single-span masonry arch bridge carries a road over the double-track Dublin-Cork railway. The abutments are of regularly-coursed rock-faced masonry blocks, with margined quoins. The arch is of semi-elliptical profile and has rusticated voussoirs (with margined edges to each block) which step into the spandrels. Its soffit is of brick, laid to the skew. A string course runs around the quoins and through the arch at spring level. The spandrels are of roughly-dressed and squared masonry laid to regular courses. A string course runs over the crown of the arch. The parapets are of dressed and squared blocks, randomly laid and with out-projecting coping stones. The angled wing walls are similar to the parapets. Signs on the parapets and abutment quoins note this bridge as Iarnród Éireann bridge no.121. The road approaches are dog-legged and ramped.

Interest Architectural; Historical; Technical

Evaluation This bridge is of architectural and technical interest on account of the quality and detailing of the stonework and skewed brick soffit to the arch. It is also of historical interest due to its association with the Great Southern & Western Railway Co. It is of local industrial heritage significance.

Rating Local **Protection** **Action** None

Associated with LAIAR-004-013

Other dbase

References

LAIAR-004-014_01 FWH 11/04/2007 NE elevation.



LAIAR-004-014_02 FWH 11/04/2007 SW elevation.



Site no LAIAR-004-015 **Kilbride Bridge**

County Laois **Townland** Kilbride **Town**
Planning Laois CC **Discovery map** 55 **Six-inch map** 4

Summary Skew arched masonry road bridge over Dublin-Cork railway line, opened by Great Southern & Western Railway Co in 1847. Soffit is of skewly-laid brickwork.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway was opened in 1847. An uncaptioned bridge is marked on the 1888 OS map and Kilbride Bridge on the 1907 edition.

Component LAIAR-004-015 1

Type Bridge (road/rail) **Function** Railway
Category Transport **Context** Infrastructure
Irish Grid 252926 209262 +/- 10m
Survey date 11/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge

Description A skewed single-span masonry arch bridge carries a road over the double-track Dublin-Cork railway. The abutments are of regularly-coursed rock-faced masonry blocks, with margined quoins. There are shallow abutments to each face, detailed as the abutments. The arch is of semi-elliptical profile and measures 9.14m orthogonally (10.20m on the skew). It is embellished with rusticated voussoirs (with margined edges to each block) which step into the spandrels. Its soffit is of brick, laid to the skew. A string course runs around the quoins and through the arch at spring level. The spandrels are of roughly-dressed and squared masonry laid to regular courses. A string course runs over the crown of the arch. The parapets are of dressed and squared blocks, randomly laid and with out-projecting coping stones. They are 9.42m apart and terminate in out-projecting piers which are continuations of the buttresses. Signs on the abutment quoins note this bridge as Iarnród Éireann bridge no.123. The road approaches are dog-legged and ramped.

Interest Architectural; Historical; Technical

Evaluation This bridge is of architectural and technical interest on account of the quality and detailing of the stonework and skewed brick soffit to the arch. It is also of historical interest due to its association with the Great Southern & Western Railway Co. It is of local industrial heritage significance.

Rating Local **Protection** **Action** None

Associated with LAIAR-004-013

Other dbase

References

LAIAR-004-015_01 FWH 11/04/2007 SW elevation.



LAIAR-004-015_02 FWH 11/04/2007 Stonework detail at right-hand side of SW elevation.



LAIAR-004-015_03 FWH 11/04/2007 Skewed brickwork detail in soffit.



Site no **LAIAR-004-017** **Commons Bridge**

County Laois **Townland** Cooltedery **Town**
Planning Laois CC **Discovery map** 55 **Six-inch map** 4

Summary A single-span pre-cast reinforced-concrete beam and slab road bridge over Portarlington-Athlone railway. This section of line, between Portarlington and Tullamore, was opened in 1854 by the Great Southern & Western Railway Co. The deck is a modern replacement on its original masonry abutments.

History This bridge is located on the Portarlington-Tullamore section of the Athlone line, opened by the Great Southern & Western Railway Company in 1854. It is cited as Commons Bridge on the 1907 OS map. The original deck was replaced with a reinforced-concrete one in the early 2000s.

Component **LAIAR-004-017 1**

Type Bridge (road/rail) **Function** Railway
Category Transport **Context** Infrastructure
Irish Grid 253730 211867 +/- 10m
Survey date 11/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge

Description A single-span concrete bridge carries a public road over the single-track Portarlington-Athlone line. The abutments are of coursed and rock-faced masonry blocks; their quoins have margined edges. The upper sections of the abutments have been replaced with pre-cast concrete pillow blocks on which rest pre-cast reinforced-concrete beams and a replacement concrete slab deck. The span is estimated to be c.9.15m (30ft). The parapets are modern and comprise roughly-dressed and coursed stone blocks. They are spaced at 8.14m and are coped with triangular-profile concrete. There is a footpath along one side of the road. A sign on the parapet notes that this is bridge 'OB A4'. The angled wing walls are of squared random rubble, laid randomly and coped with dressed stone blocks. They terminate in dressed stone piers coped with chamfered stone caps. The approach roads are ramped. Their rubble masonry walls have been rebuilt and partly replaced with concrete blocks.

Interest None

Evaluation The replacement deck has diminished this bridge's architectural and historical integrity and it is now of no special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with LAIAR-004-021

Other dbase

References

LAIAR-004-017_01 FWH 11/04/2007 NW elevation.



Site no LAIAR-004-022

County	Laois; Offaly	Townland	Cooltedery; Co Offaly	Town	
Planning	Laois CC; Offaly CC	Discovery map	55	Six-inch map	4

Summary Skew triple-span pre-cast reinforced-concrete beam and slab bridge carries Portarlington-Athlone railway line over River Barrow at county boundary. This section of line, between Portarlington and Tullamore, was opened in 1854 by the Great Southern & Western Railway Co. The deck is a 1984 replacement of a previous one and sits on its original masonry abutments and piers.

History This section of line, between Portarlington and Tullamore, was opened in 1854 by the Great Southern & Western Railway Company. The original deck, which was probably on metal girders, was replaced with a reinforced-concrete one in 1984.

Component LAIAR-004-022 1

Type	Bridge (rail/river)	Function	Railway		
Category	Transport	Context	Infrastructure		
Irish Grid	253627 211938		+/- 10m		
Survey date	11/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Rail bridge
Description	A skew triple-span concrete rail bridge over the River Barrow at boundary with Co Offaly. The abutments and piers are of rock-faced limestone blocks, regularly coursed and with rusticated and margined quoins. The piers have similarly detailed triangular cutwaters at both ends. The replacement deck comprises multiple pre-cast reinforced-concrete beams set on concrete pillow blocks on the original stonework. Its spans are, from the left (Offaly) to right (Laois) bank: 5.54m, 13.00m and 5.47m (as measured at right angles to the abutments). It is c.5.0m wide, but the abutments are approximately twice as deep to allow for a later doubling of the track (this never happened). Triple-bar tubular steel railings continue as limestone block walls terminating in out-projecting piers. The end spans facilitate the movement of animals under the line.				

Interest Historical; Setting

Evaluation This is a relatively substantial bridge, and a significant feature of the riverscape. Its architectural integrity is diminished by the deck's replacement, but its historical association with the Great Southern & Western Railway remains. It is of local industrial heritage interest.

Rating	Local	Protection		Action	None
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Associated with LAIAR-004-021

Other dbase OFIAR-034-012

References

Duffy, J. Barrow Bridges (Tullow: John Duffy, 2007).

Page(s) 95

LAIAR-004-022_01 FWH 17/06/2004 Upstream (SW) elevation, from left (Offaly) bank.



LAIAR-004-022_02 FWH 11/04/2007 Upstream (SW) elevation, from right (Laois) bank.



LAIAR-004-022_03 FWH 17/06/2004 Downstream (NE) elevation, from right (Laois) bank.



LAIAR-004-022_04 FWH 17/06/2004 Abutment detail - much wider than the deck.



LAIAR-004-022_05 FWH 17/06/2004 Pier and cutwater detailing. Note concrete pillow block over.



Site no LAIAR-004-023

County Laois **Townland** Tinnakill (Portnahinch By) **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 4

Summary A masonry slab culvert of c.1828 carries a drain under the Mountmellick Branch of Grand Canal.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. This aqueduct is cited on the 1839 and 1888 OS maps and shown on the 1907 edition.

Component LAIAR-004-023 1

Type Bridge (canal/river) **Function** Inland waterway
Category Transport **Context** Infrastructure
Irish Grid 250073 209157 +/- 10m
Survey date 30/08/2006 **Surveyor** Fred Hamond
Remains Complete **Condition** Fair **Current use** Culvert

Description A culvert carries a drainage channel under the Mountmellick Canal. Comprises flat stone lintels over abutments, 60cm wide by c.90cm high. The first two metres at the SE end comprise a slightly higher shallow segmental arch. The spandrels and wing walls are of regularly coursed squared limestone rubble.

Interest Historical

Evaluation Of local industrial heritage significance due to its historical association with the Mountmellick Canal.

Rating Local **Protection** **Action** None

Associated with LAIAR-004-005

Other dbase Mountmellick Canal F302

References

LAIAR-004-023_01 FWH 30/08/2006 South-east face of culvert under canal.



Site no LAIAR-004-024

County Laois **Townland** Coolnavarnoge and Coolaghy **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 4

Summary A culvert of c.1828 carries a minor tributary of the River Barrow under a feeder to the Mountmellick Branch of the Grand Canal.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. This aqueduct on one of its feeders is shown on the 1839 and 1888 OS maps and is explicitly cited on the 1907 edition.

Component LAIAR-004-024 1

Type Bridge (canal/river) **Function** Inland waterway
Category Transport **Context** Infrastructure
Irish Grid 251893 209923 +/- 10m
Survey date 29/06/2006 **Surveyor** Fred Hamond
Remains Substantial remains **Condition** Poor **Current use** Culvert
Description An arched culvert carries a tributary of the River Barrow under a now-dry Mountmellick Canal feeder channel immediately SW of a minor road. Of random rubble construction. The arch is of semicircular profile, with brick voussoirs, and spans 76cm. The soffit directly under the feeder channel has been deliberately breached so that any water now drains into the stream running underneath the feeder. The latter passes under the road north-east of the aqueduct in a 30cm wide random rubble culvert; its north-east end has been removed.

Interest Historical

Evaluation Of local industrial heritage significance due to its historical association with the Mountmellick Canal.

Rating Local **Protection** **Action** None

Associated with LAIAR-004-018

Other dbase Mountmellick Canal F208a

References

LAIAR-004-024_01 FWH 29/06/2006 South-west elevation of canal culvert.



Site no LAIAR-004-027

County	Laois	Townland	Kilbride	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	4

Summary Masonry arch accommodation bridge over Dublin-Cork railway line, opened by Great Southern & Western Railway Co in 1847.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway was opened in 1847. An uncaptioned bridge is marked on the 1888 and 1907 OS maps. The deck is a modern replacement.

Component LAIAR-004-027 1

Type	Bridge (road/rail)	Function	Railway		
Category	Transport	Context	Infrastructure		
Irish Grid	253040 209428	+/- 10m			
Survey date	11/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A single-span concrete bridge carries an accommodation track over the double-track Dublin-Cork railway. The abutments are of regularly-coursed rock-faced masonry blocks, with margined quoins. The tops of the abutments have been replaced with concrete pillow blocks to take pre-cast reinforced-concrete beams over which is a replacement concrete slab deck. The parapets, which are 3.93m apart, are of concrete brick and are coped with concrete. The original walls survive at each end and are of coursed and squared masonry blocks. The ends of these walls are rusticated and margined, and have moulded coping stones. A string course runs along the base of these walls on their outside faces. The approaches are ramped.				

Interest Architectural

Evaluation The detailing on the stonework on the end walls to this bridge is of note. However, the bridge's overall integrity has been compromised by the replacement deck and it is no longer of special industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with LAIAR-004-013

Other dbase

References

LAIAR-004-027_01 FWH 11/04/2007 SW elevation.



LAIAR-004-027_02 FWH 11/04/2007 Moulded coping stone at end of approach wall to parapet.



Site no LAIAR-004-028

Clonterry Bridge

County	Laois; Offaly	Townland	Clonterry; Co Offaly	Town	
Planning	Laois CC; Offaly CC	Discovery map	54	Six-inch map	4

Summary A twin-span concrete road bridge of late 19th/early 20th century date over River Barrow at county boundary. The original metal girder and timber superstructure was replaced with a reinforced-concrete beam and slab deck c.2000.

History First shown on the 1909 OS 25-inch map as a narrow bridge. Not on the previous edition of 1888. The deck was replaced c.2000. An Offaly CC bridge record (undated) notes that the previous bridge comprised two spans - 8.22m and 8.35m. Each had two 40cm deep I-beams with 2.75m wide timber railway sleepers over.

Component LAIAR-004-028 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	247839 209875		+/- 10m		
Survey date	11/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A twin-span concrete bridge carries a public road over the River Barrow on the boundary with Co Offaly. The abutments and pier are of concrete and were part of the original structure (which had timber sleepers over metal girders). The pier has triangular cutwaters at each end. The replacement deck comprises eight pre-cast reinforced-concrete beams under a concrete slab deck; both spans are 8.22m wide. The parapet comprises three-bar galvanised steel railings set 2.93m apart and terminating in concrete piers.				

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase OFIAR-033-013

References

Duffy, J. Barrow Bridges (Tullow: John Duffy, 2007).

Page(s) 91

LAIAR-004-028_01 FWH 17/06/2004 Upstream (W) elevation.



Site no LAIAR-004-029

County	Laois; Offaly	Townland	Ballymorris; Co Offaly	Town	
Planning	Laois CC; Offaly CC	Discovery map	54	Six-inch map	4

Summary Site of accommodation bridge over River Barrow to Lansdowne Park House. On boundary with Co Offaly.

History A bridge leading to Lansdowne Park House is shown on the 1839, 1888 and 1907 OS maps.

Component LAIAR-004-029 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Settlement		
Irish Grid	251754 211198 +/- 10m				
Survey date	11/04/2007	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	No trace of this bridge over the River Barrow survives. Probably cleared during dredging operations after Lansdowne Park House was abandoned.				

Interest None

Evaluation No industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with

Other dbase

References

Site no LAIAR-004-030

County	Laois	Townland	Coolnavarnoge and Coolaghy; Kilbride	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	4

Summary Site of a masonry culvert of 1847 carrying a feeder to the Mountmellick Canal under a road.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. This bridge over one of the canal's feeders was constructed when the road was realigned after the opening of the Portarlington-Portlaoise railway in 1847. The bridge is shown on the 1888 and 1907 OS maps. The road has been realigned once again in the recent past.

Component LAIAR-004-030 1

Type	Bridge (road/canal feeder)	Function	Inland waterway		
Category	Transport	Context	Infrastructure		
Irish Grid	252877 209148	+/- 10m			
Survey date	30/08/2006	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	The road has been realigned and the earlier stretch removed along with this culvert.				

Interest None

Evaluation No industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with

Other dbase

References

Site no LAIAR-004-031

County	Laois	Townland	Coolnavarnoge and Coolaghy; Kilbride	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	4

Summary An arched masonry culvert of 1847 carrying a feeder to the Mountmellick Canal under the Dublin-Cork railway.

History This bridge over a feeder to the Mountmellick Canal was erected by the Great Southern and Western Railway Company. This section of the railway was opened in 1847. An uncaptioned bridge is marked on the 1888 and 1907 OS maps.

Component LAIAR-004-031 1

Type	Bridge (rail/canal feeder)	Function	Inland waterway		
Category	Transport	Context	Infrastructure		
Irish Grid	252858 209161		+/- 10m		
Survey date	30/08/2006	Surveyor	Fred Hamond		
Remains	Complete	Condition	Fair	Current use	Culvert
Description	A culvert conveys a feeder to the Mountmellick Canal under a railway embankment. The arch is of segmental profile and spans 1.52m. Voussoirs are of dressed masonry but otherwise of random rubble throughout. Dressed string course runs across the crown. No parapet. Slightly curved as it runs under the embankment.				

Interest Historical

Evaluation Of local industrial heritage significance due to its historical association with the Mountmellick Canal.

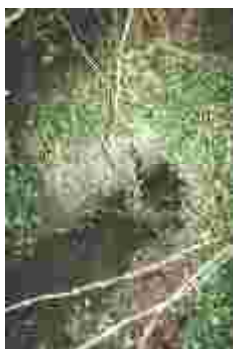
Rating	Local	Protection		Action	None
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Associated with

Other dbase

References

LAIAR-004-031_01 FWH 30/08/2006 South-east face of culvert under railway embankment.



Site no LAIAR-004-032

County	Laois	Townland	Coolnavarnoge and Coolaghy; Kilbride	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	4

Summary Skewed twin-span masonry arch culvert of c.1828 carrying a feeder to the Mountmellick Canal under a road.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. This bridge over one of the canal's feeders is shown on the 1839, 1888 and 1907 OS maps.

Component LAIAR-004-032 1

Type	Bridge (road/canal feeder)	Function	Inland waterway		
Category	Transport	Context	Infrastructure		
Irish Grid	252236 209465		+/- 10m		
Survey date	29/06/2006	Surveyor	Fred Hamond		
Remains	Complete	Condition	Fair	Current use	Road bridge

Description A highly skewed twin-arch culvert carries a public road over a canal feeder to the Mountmellick Canal south-west of Skeagh Crossroads. Of random rubble construction. Arches are of segmental profile and have finely dressed voussoirs. Pier is square-cut, not angled. Each arch is 90cm wide (measured orthogonally). Parapet is coped with limestone blocks; some mass-concrete repairs. The intake at the upstream end of the south-west arch has been infilled during landscaping of the garden of an adjoining house. Heavily vegetated on upstream face.

Interest Historical

Evaluation Of local industrial heritage significance due to its historical association with the Mountmellick Canal.

Rating	Local	Protection		Action	None
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Associated with

Other dbase

References

LAIAR-004-032_01 FWH 29/06/2006 Downstream (north-west) elevation.



LAIAR-004-032_02 FWH 29/06/2006 Voussoir detail on downstream face.



Site no LAIAR-005-001 **Barrow Bridge**

County Laois; Offaly **Townland** Cooltedery; Co Offaly **Town** Portarlinton

Planning Laois CC; Offaly CC **Discovery map** 55 **Six-inch map** 5

Summary An eight-arch masonry road bridge of 18th/early 19th century date over the River Barrow on boundary with Co Offaly. Widened on upstream side in 19th century. Cantilevered footpaths added in later 20th century.

History A wooden bridge is shown on a 1678 plan of Portarlinton. A bridge is shown on Taylor & Skinner's 1783 map and again on the 1838 OS map. It is captioned Barrow Bridge on the 1888 and 1907 editions. It was widened on its upstream side in the 19th century. Cantilevered footpaths and metal railings were added around 1970.

Component LAIAR-005-001 1

Type Bridge (road/river) **Function** Road & pedestrian

Category Transport **Context** Infrastructure

Irish Grid 254024 212651 +/- 10m

Survey date 11/04/2007 **Surveyor** Fred Hamond

Remains Complete **Condition** Good **Current use** Road bridge

Description An eight-arch masonry bridge carries the Portarlinton-Tullamore road over the Barrow River at boundary with Co Offaly. Of random rubble construction throughout. The abutments and piers are underpinned with concrete. Triangular cutwaters at both ends of piers. Those on the upstream (south) side are of dressed limestone blocks, randomly laid; their tops have been truncated above arch spring level (to make way for cantilevered footpaths). Those to the downstream side rise to just below the arch spring line. The arches are of semicircular profile. Their spans, from the left (Offaly) to right (Laois) bank are: 4.12m, 3.63m, 4.34m, 4.59m, 4.60m, 3.84m, 3.84m, 3.55m. Only the five on the left bank (four of which are in Co Offaly) are now in normal use. All the soffits have been gunited. The arch rings to their upstream faces are of dressed limestone blocks, whereas those on the downstream side are of split random rubble (this suggests that the upstream side is later, i.e. a widening). Reinforced-concrete footpaths are cantilevered out on each side, extending the bridge's width from 7.65m to 11.35m. The original parapets have been replaced with vertical steel railings. The deck is slightly curved.

Interest Archaeological; Historical; Setting

Evaluation This bridge is of historical interest in demonstrating the growth of road traffic from the 1700s onwards. It is the only eight-arch masonry bridge in the county. Although the soffits are gunited, the style of the cutwaters and arch rings indicate that the bridge was widened on its upstream side, probably in the 1800s. It was widened yet again in the later 1900s to accommodate footpaths on both sides. It is also a significant landscape feature in the town. Unfortunately, its visual disfigurement by concrete underpinning, guniting and cantilevered footpaths has greatly diminished its architectural character and it is now of local rather than regional heritage significance. It is in the Record of Monuments & Places (LA005-031002-) for archaeological reasons.

Rating Local **Protection** RMP LA005-031002- **Action** None

Associated with

Other dbase LA Bridge R018; OFIAR-034-005; SMR LA005-031002-

References

- Archaeological Survey of Ireland: Sites & Monuments Record print-out. Page(s)
- Duffy, J. Barrow Bridges (Tullow: John Duffy, 2007). Page(s) 96-97
- Lawrence Photographic Collection, Laois County Library (Local Studies Dept), Portlaoise. Page(s) WL 7464
- Orford, D. Those Were the Days: Memories of Portarlington, 1900 to 1999 (Portarlington: Portarlington Historical Society). Page(s)

LAIAR-005-001_01 FWH 17/06/2004 Upstream (S) elevation.



LAIAR-005-001_02 FWH 11/04/2007 Downstream (N) elevation from left (Offaly) bank.



LAIAR-005-001_03 FWH 17/06/2004 Downstream (N) elevation from right (Laois) bank.



LAIAR-005-001_04 FWH 17/06/2004 Pier and cantilevered footpath detail on upstream side.



Site no **LAIAR-005-002** **Spa Bridge**

County Laois **Townland** Cooltedery; Droughill **Town** Portarlinton

Planning Laois CC **Discovery map** 55 **Six-inch map** 5

Summary An 18th century five-arch masonry road bridge over the River Barrow. Widened on downstream side with five concrete arches in 2000.

History A bridge is shown on Taylor & Skinner's 1783 map. It is not on a 1776 map, so must date to around 1780. It is shown on the 1838, 1888 and 1907 OS maps. It is captioned Spa Bridge on the 1907 edition. It was widened on its downstream side in 1999-2000.

Component **LAIAR-005-002 1**

Type Bridge (road/river) Function Road & pedestrian

Category Transport Context Infrastructure

Irish Grid 254266 212858 +/- 10m

Survey date 11/04/2007 Surveyor Fred Hamond

Remains Complete Condition Excellent Current use Road bridge

Description A five-arch masonry bridge carries a public road over the River Barrow on the northern outskirts of Portarlinton. Only the three middle arches are in normal use. The upstream (west) side is of random rubble construction throughout. The stone abutments and piers are 7.21m deep and have concrete footings. The upstream ends of the piers have triangular cutwaters rising to just above arch spring level. The arches rise to the centre of the bridge and are of semicircular profile. Their voussoirs are of dressed and split rubble masonry. The spans' widths from the left to right bank are: 4.49m, 5.46m, 6.20m, 5.55m, and 4.63m. The spandrels and parapet are of random rubble and are 1999 rebuilds of the original stonework. The parapet coping stones are laid on their edges.

The bridge has been widened on its downstream (east) side by 3.6-4.0m. The abutments and piers of this added section are of concrete. The cutwaters on the downstream ends of the piers are of random rubble and rise to arch spring level. The arches are of pre-cast reinforced-concrete units of similar size and profile to the stone arches; they are positioned such that the downstream voussoirs of the masonry section are visible. The width of their spans from the left to right bank are: 4.79m, 5.61m, 6.32m, 5.91m, and 5.03m. The spandrels and parapets are of random rubble. The old and new parapets are spaced at 8.24m. The deck is curved and footpaths run along both sides.

At the right-bank end of the upstream side is a stone plaque commemorating the bridge's reopening by Joe Dunne, Chairman of Laois County Council, on 9 June 2000.

Interest Archaeological; Historical; Setting

Evaluation This bridge is of historical interest in demonstrating the growth of road traffic in the later 1900s. It is also a significant landscape feature in the town. Of the original structure, only the abutments, piers and arch rings survive intact, the other components having been rebuilt when the bridge was widened in 1999. This diminution of the bridge's overall integrity reduces its industrial heritage significance from regional to local. It is in the Record of Monuments & Places (LA005-031008-) for archaeological reasons.

Rating Local **Protection** RMP LA005-031008- **Action** None

Associated with**Other dbase** SMR LA005-031008-

References

- Archaeological Survey of Ireland: Sites & Monuments Record print-out. Page(s)
- Duffy, J. Barrow Bridges (Tullow: John Duffy, 2007). Page(s) 98-99
- Orford, D. Those Were the Days: Memories of Portarlinton, 1900 to 1999 (Portarlinton: Portarlinton Historical Society). Page(s) 240
- Somers, P. 'Portarlinton's Spa Bridge re-opened'. In The Leinster Express, 17 June 2000. Page(s) 7
- 'Work continues on Spa bridge spanning the Barrow'. In The Leinster Express, 3 July 1999. Page(s) 33

LAIAR-005-002_01 FWH 11/04/2007 Upstream (W) elevation.



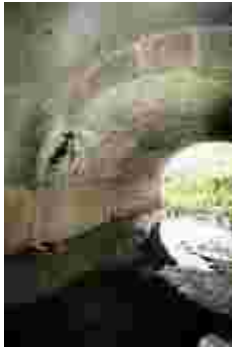
LAIAR-005-002_02 FWH 11/04/2007 Downstream (E) elevation.



LAIAR-005-002_03 FWH 11/04/2007 Voussoir detail on downstream side and modern extension.



LAIAR-005-002_04 FWH 11/04/2007 Voussoir detail on downstream side and modern extension.



LAIAR-005-002_05 FWH 11/04/2007 Modern concrete arch detail.



LAIAR-005-002_06 FWH 11/04/2007 Plaque at upstream right-bank end of parapet.



Site no LAIAR-005-004

County Laois **Townland** Cooltedery **Town** Portarlinton

Planning Laois CC **Discovery map** 55 **Six-inch map** 5

Summary A single-span concrete bridge carries the Portarlinton-Athlone railway over an accommodation track. This section of line, between Portarlinton and Tullamore, was opened in 1854 by the Great Southern & Western Railway Co. The deck is a 20th century replacement on its original masonry abutments.

History This bridge is located on the Portarlinton-Tullamore section of the line to Athlone, opened by the Great Southern & Western Railway Company in 1854. The present span is a 20th century replacement on the original abutments.

Component LAIAR-005-004 1

Type Bridge (rail/road) **Function** Railway

Category Transport **Context** Infrastructure

Irish Grid 254066 211489 +/- 10m

Survey date 11/04/2007 **Surveyor** Fred Hamond

Remains Complete **Condition** Good **Current use** Rail bridge

Description A single-span concrete bridge carries the single-track Portarlinton-Athlone line over an accommodation track. The abutments are of squared random rubble, laid randomly, and are embellished with rusticated and margined quoins; they are 8.78m wide. The span comprises a replacement reinforced-concrete slab deck measuring 3.65m between the abutments by 4.72m deep (the much wider abutments allowed for track doubling which never took place). The parapets are of concrete. A timber footplates runs across the north-east face of the bridge. The sloping angled wing walls are of identical construction to the abutments. They are coped with dressed stone blocks and terminate in square piers.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with LAIAR-005-003

Other dbase Mountmellick Canal F925

References

LAIAR-005-004_01 FWH 11/04/2007 SW elevation.



Site no LAIAR-005-005

County Laois **Townland** Cooltedery **Town** Portarlington

Planning Laois CC **Discovery map** 55 **Six-inch map** 5

Summary A skew metal girder bridge carries the Portarlington-Athlone railway over a public road. This section of line, between Portarlington and Tullamore, was opened in 1854 by the Great Southern & Western Railway Co. The deck is a 20th century replacement on its original masonry abutments.

History This bridge is located on the Portarlington-Tullamore section of the line to Athlone, opened by the Great Southern & Western Railway Company in 1854. The present span is a 20th century replacement on the original abutments.

Component LAIAR-005-005 1

Type Bridge (rail/road) **Function** Railway

Category Transport **Context** Infrastructure

Irish Grid 254142 211360 +/- 10m

Survey date 11/04/2007 **Surveyor** Fred Hamond

Remains Complete **Condition** Good **Current use** Rail bridge

Description A highly skewed single-span metal girder bridge carries the single-track Portarlington-Athlone line over a public road. The abutments are of rock-faced masonry blocks, laid to regular courses and embellished with quoins. They are 7.57m apart, as measured orthogonally, and 10.72m on the skew. Their width is such that doubling of the line could have taken place (although it did not). The span, which is a replacement, comprises two substantial steel plate girders between the bottom flanges of which is metal troughing. The curved and sloping wing walls are of squared random rubble, coped with masonry blocks and terminating in dressed stone piers. There are bracketed timber footplates on the outsides of the girders, with two-bar steel railings. A sign on the abutment notes that this is Iarnród Éireann bridge no.406.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with LAIAR-005-003

Other dbase Mountmellick Canal F924

References

LAIAR-005-005_01 FWH 29/06/2006 SW elevation.



LAIAR-005-005_02 FWH 11/04/2007 NE elevation.



LAIAR-005-005_03 FWH 11/04/2007 Deck and east abutment detail, from NW.



LAIAR-005-005_04 FWH 11/04/2007 NW abutment quoin detail.



Site no LAIAR-005-006

County Laois **Townland** Cooltedery **Town** Portarlington

Planning Laois CC **Discovery map** 55 **Six-inch map** 5

Summary Triple-span bridge of c.1854, originally carrying the Portarlington-Athlone Branch of Great Southern & Western railway over Mountmellick Branch of Grand Canal. The deck was replaced in steel in the 20th century. After the canal's closure in 1960, a road was built along the line of the canal.

History This bridge is located on the Portarlington-Tullamore section of the line to Athlone, opened by the Great Southern & Western Railway Company in 1854. It is shown in the 1888 and 1907 OS maps. The deck was probably replaced in 1900s (but prior to 1953 as the present steel girder span is shown in a photograph of that year). The canal has since been infilled and a road constructed.

Component LAIAR-005-006 1

Type Bridge (rail/canal) **Function** Railway

Category Transport **Context** Infrastructure

Irish Grid 254217 211173 +/- 10m

Survey date 29/06/2006 **Surveyor** Fred Hamond

Remains Complete **Condition** Good **Current use** Rail bridge

Description A slightly skew triple-span metal girder bridge carries the single-track railway line from Portarlington to Tullamore over the former canal, now infilled to create a bypass road around Portarlington. The abutments and piers are of rock-faced stone blocks laid to courses and embellished with tooled edges. The towpath which formerly ran through the south side of the middle span has been removed and the pier faced with cement. Tow rope marks are still visible at both ends. The deck, which is a replacement, comprises plate girders along each side, with metal troughing between. The north span runs over a field accommodation track. The south span is partly infilled with soil. The north, middle and south spans measure 3.66, 9.91m and 3.65m respectively (all measured orthogonally). Although the deck is only 4.30m in depth, the abutments are 9.45m (on the skew), i.e. the bridge was built to accommodate two tracks, but only one was laid. Curved wing walls of squared random rubble coped with dressed stones and terminating in square piers. Iarnród Éireann bridge no.551.

Interest Rarity; Setting

Evaluation Only the abutments and piers are original, the deck having been replaced. The bridge is of some interest in being the only example of a railway bridge over this branch of the Grand Canal. This is now only evident in the rope marks on the quoins. It is also a prominent landscape feature along the infilled canal. Of local industrial heritage significance.

Rating Local **Protection** **Action** None

Associated with LAIAR-005-003; LAIAR-005-007

Other dbase IWAI MMC 110; Mountmellick Canal F203

References

Orford, D. Those Were the Days: Memories of Portarlington, 1900 to 1999 Page(s) 99
(Portarlington: Portarlington Historical Society).

LAIAR-005-006_01 FWH 29/06/2006 General view from west.



LAIAR-005-006_02 FWH 29/06/2006 General view from east.



LAIAR-005-006_03 FWH 29/06/2006 Tow rope marks on south-east pier of middle span.



LAIAR-005-006_04 FWH 29/06/2006 Deck underside. Note transverse metal troughing.



Site no LAIAR-005-008 **Ballymorris Bridge**

County Laois **Townland** Ballymorris; Cooltedery **Town** Portarlinton

Planning Laois CC **Discovery map** 55 **Six-inch map** 5

Summary Site of a road bridge of c.1828 over the Mountmellick Branch of the Grand Canal.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. This bridge is shown on the 1838 and 1888 OS maps and cited as Ballymorris Bridge on the 1907 edition.

Component LAIAR-005-008 1

Type Bridge (road/canal) Function Inland waterway

Category Transport Context Infrastructure

Irish Grid 254069 211143 +/- 10m

Survey date 29/06/2006 Surveyor Fred Hamond

Remains No visible remains Condition N/A Current use N/A

Description The bridge carrying a public road over the Mountmellick Canal was demolished when the bed was infilled to create road by-pass around Portarlinton.

Interest None

Evaluation No industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with LAIAR-005-007

Other dbase IWAI MMC 120; Mountmellick Canal F204

References

Site no LAIAR-005-009 **Swing Bridge; {Draw Bridge}**

County Laois **Townland** Cooltedery **Town** Portarlinton

Planning Laois CC **Discovery map** 55 **Six-inch map** 5

Summary Site of swivel road bridge of c.1828 over Mountmellick Branch of Grand Canal.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. 'Draw Bridge' is cited on the 1838 and 1888 OS maps, and 'Swing Bridge' on the 1907 edition. Old photographs show it to have swivelled around a pivot at one end.

Component LAIAR-005-009 1

Type Bridge (road/canal) **Function** Inland waterway

Category Transport **Context** Infrastructure

Irish Grid 254732 211165 +/- 10m

Survey date 29/06/2006 **Surveyor** Fred Hamond

Remains No visible remains **Condition** N/A **Current use** N/A

Description The bridge carrying a public road over the Mountmellick Canal has been completely cleared to make way for a wider road over the infilled canal bed.

Interest None

Evaluation No industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with LAIAR-005-007

Other dbase IWAI MMC 100; Mountmellick Canal F202

References

Site no **LAIAR-005-011** **Lock Bridge**

County Laois **Townland** Bracklone **Town** Portarlington

Planning Laois CC **Discovery map** 55 **Six-inch map** 5

Summary Site of road bridge of c.1828 over the Mountmellick Canal. There are also the remains of the 2nd lock, lock-keeper's house and goods store at this location.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. This bridge is shown on the 1838 and 1888 OS maps and cited as Lock Bridge on the 1907 edition.

Component **LAIAR-005-011 4**

Type Bridge (road/canal) Function Inland waterway

Category Transport Context Infrastructure

Irish Grid 255411 211651 +/- 10m

Survey date 22/08/2006 Surveyor Fred Hamond

Remains No visible remains Condition N/A Current use N/A

Description This bridge carried a public road over the north-east end of the 2nd lock on the Mountmellick Canal. It was demolished to make way for a new road running directly across the infilled canal bed.

Interest None

Evaluation No industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with LAIAR-005-007

Other dbase IWAI MMC 080; Mountmellick Canal F201b

References

Champ, L. 'Memories of Portarlington's Canal'. In Orford, D. Those Were the Days: Memories of Portarlington, 1900 to 1999 (Portarlington: Portarlington Historical Society). Page(s) 38

Delany, R. The Grand Canal of Ireland (Newton Abbot: David and Charles, 1995). Page(s) 216

Mathews, R. Portarlington: the Inside Story. Page(s) 126

Site no **LAIAR-005-014** **Lea Bridge**

County Laois **Townland** Lea **Town**
Planning Laois CC **Discovery map** 55 **Six-inch map** 5

Summary Site of an arched masonry road bridge of c.1828 over the Mountmellick Branch of the Grand Canal.

History Lea Bridge is cited on the 1838, 1888 and 1907 OS maps.

Component **LAIAR-005-014 1**

Type Bridge (road/canal) Function Inland waterway
Category Transport Context Infrastructure
Irish Grid 257418 211844 +/- 10m
Survey date 22/08/2006 Surveyor Fred Hamond
Remains No visible remains Condition N/A Current use N/A
Description Site of bridge carrying public road over canal. Demolished c.20 years ago and canal infilled hereabouts for agricultural use. Superseded by road over the infilled bed a short distance west of this former bridge.

Interest None

Evaluation No industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with LAIAR-005-007

Other dbase IWAI MMC 070; Mountmellick Canal F106

References

Site no LAIAR-005-016

Bergin's Bridge

County	Laois	Townland	Loughmansland Glebe	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	5

Summary An arched masonry road bridge of c.1828 over the Mountmellick Branch of the Grand Canal.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. Bergin's Bridge is cited on the 1838, 1888 and 1907 OS maps.

Component LAIAR-005-016 1

Type	Bridge (road/canal)	Function	Inland waterway		
Category	Transport	Context	Infrastructure		
Irish Grid	258303 211345 +/- 10m				
Survey date	22/08/2006	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge

Description A masonry arch bridge carries a public road over a now-infilled section of the Mountmellick Branch of Grand Canal. Of randomly coursed squared limestone rubble. The arch is of semicircular profile and spans 6.68m. Its soffit is of squared rubble and its voussoirs are of finely dressed stone. A 1.70m wide towpath runs through its west side (the channel is 4.98m wide). Both faces of the bridge curve outwards and are embellished with a finely dressed string course which continues under the outsides of the parapets. The parapets are coped with dressed limestone blocks and their inside faces are of random rubble; they are spaced at 6.42m. Humped deck. No datestones. Ivy overgrowth.

Interest Architectural; Historical

Evaluation This unaltered bridge is of architectural and historical interest, being typical of the bridges along this branch of the Grand Canal. It also has group value in the context of the overall canal. It is of regional heritage significance and merits inclusion in the Record of Protected Structures.

Rating	Regional	Protection		Action	RPS
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Associated with LAIAR-005-007

Other dbase IWAI MMC 060; LA Bridge R020; Mountmellick Canal F105

References

LAIAR-005-016_01 FWH 22/08/2006 North face of bridge.



LAIAR-005-016_02 FWH 22/08/2006 Rubbish dumping underneath arch of bridge.



Site no LAIAR-005-017 **Wheelahan's Bridge**

County	Laois	Townland	Clonanny	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	5

Summary An arched masonry road bridge of c.1828 over the Mountmellick Branch of the Grand Canal. Now by-passed by a new road and used for local site access only.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. Wheelahan's Bridge is cited on the 1838, 1888 and 1907 OS maps.

Component LAIAR-005-017 1

Type	Bridge (road/canal)	Function	Inland waterway		
Category	Transport	Context	Infrastructure		
Irish Grid	259637 211148 +/- 10m				
Survey date	22/08/2006	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A masonry arch bridge carries a public road over a now-infilled section of Mountmellick Branch of Grand Canal. Of randomly coursed squared limestone rubble. The slightly skew arch is of semicircular profile and spans 6.69m. Its soffit is of squared rubble (laid orthogonally) and its voussoirs are of finely dressed stone. A 1.74m wide towpath runs through its south side (the channel is 4.95m wide). There are towrope marks at both ends of the arch on this side. Both faces of the bridge curve outwards and are embellished with a finely dressed string course which continues under the outsides of the parapets. The parapets are coped with dressed limestone blocks and their inside faces are of random rubble; they are spaced at 6.37m. Humped deck. No datestones. Ivy overgrowth on south face. Dogleg approach roads. Now by-passed to north by new road and used for access purposes only.				

Interest Architectural; Historical

Evaluation This unaltered bridge is of typical design for this canal branch and is of architectural and historical interest. Its current status as a Protected Structure (LA 547) is merited because of its regional heritage significance.

Rating	Regional	Protection	RPS LA 547	Action	None (in RPS)
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Associated with LAIAR-005-007

Other dbase IWAI MMC 050; Mountmellick Canal F103; NIAH 12800551 (R)

References

Survey of the Architectural Heritage of County Laois (Dublin: NIAH, 2002). Page(s)

LAIAR-005-017_01 FWH 22/08/2006 North elevation of bridge.



LAIAR-005-017_02 FWH 22/08/2006 North elevation of bridge.



Site no LAIAR-005-018

County	Laois	Townland	Clonanny	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	5

Summary Site of culvert of c.1828 carrying a drainage channel under the Mountmellick Branch of the Grand Canal.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. This aqueduct is cited on the 1838, 1888 and 1907 OS maps.

Component LAIAR-005-018 1

Type	Bridge (canal/river)	Function	Inland waterway		
Category	Transport	Context	Infrastructure		
Irish Grid	259974 211205		+/- 10m		
Survey date	22/08/2006	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	Site of a culvert which formerly carried a drainage channel under a now-demolished section of the canal.				

Interest None

Evaluation No industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with LAIAR-005-007

Other dbase Mountmellick Canal F102

References

Site no LAIAR-005-020

County	Laois; Offaly	Townland	Lea; Co Offaly	Town	
Planning	Laois CC; Offaly CC	Discovery map	55	Six-inch map	5

Summary A triple-span metal beam and concrete slab foot bridge, over River Barrow at boundary with Co Kildare. The present structure may be a c.1930 replacement of an earlier foot bridge, erected during the Barrow Drainage Works.

History A foot bridge is first shown on the 1907 OS map, beside Caughamore Ford. The present concrete foot bridge may be a replacement erected during the Barrow Drainage Works, 1926-34.

Component LAIAR-005-020 1

Type	Bridge (foot/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	258208 212306		+/- 10m		
Survey date	23/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Foot bridge
Description	A triple-span metal beam and concrete bridge carries a footpath over the River Barrow on the boundary with Co Kildare. The abutments and piers are of mass concrete, cast in-situ. The piers have triangular cutwaters to both ends. The concrete slab deck, which is 76cm side, has been cast in situ between two longitudinal metal beams. Their spans, from the left- to right bank, are: 6.01m, 6.26m, and 6.02m. Vestiges of the metal shuttering, of shallow segmental profile, survives on the deck's underside. There is a single-bar tubular metal railing along the downstream (east) side of the deck. There is a ford for agricultural vehicles immediately upstream.				

Interest None

Evaluation No special industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with

Other dbase

References

Hogan, T.L. 'River Barrow Drainage', in Proc Inst Civil Engineers Ireland, Page(s) 137-168
vol.65 (1939).

LAIAR-005-020_01 FWH 23/04/2007 Upstream elevation.



LAIAR-005-020_02 FWH 23/04/2007 Underside of deck (left-bank span).



LAIAR-005-020_03 FWH 23/04/2007 Tractor crossing ford on upstream side of bridge.



Site no LAIAR-005-021 **Scaravagh Bridge; {Scorraus Bridge}**

County Laois; Offaly **Townland** Inchacooly; Co Offaly **Town**
Planning Laois CC; Offaly CC **Discovery map** 55 **Six-inch map** 5

Summary A twin-arch masonry road bridge of 18th/early 19th century date over a former course of the River Barrow on the boundary with Co Kildare. This watercourse is now a tributary of the Black River.

History Scorraus Bridge is captioned on the 1838 and 1888 OS maps over the old course of the River Barrow. It is named as Scaravagh Bridge on the 1907 edition.

Component LAIAR-005-021 1

Type Bridge (road/river) **Function** Road & pedestrian
Category Transport **Context** Infrastructure
Irish Grid 260046 213495 +/- 10m
Survey date 23/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Fair **Current use** Road bridge
Description A twin-span arched masonry bridge carries a public road over a former course of the River Barrow at the boundary with Co Kildare (this now forms a tributary of the Black River). It is of random rubble construction throughout. Only the right-bank (east) arch is in normal use. The pier, to which an angled concrete cutwater has been added on its upstream (south) end, is now partly buried in the bank. There are also concrete footings to the right-bank abutment and pier. The left and right bank arches are of semicircular profile and span 1.81m and 1.87m respectively. The parapets are 6.30m apart. Sections of both parapets are now missing. A pipe is carried across the upstream side, supported on concrete block piers.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase LA Bridge C013

References

LAIAR-005-021_01 FWH 23/04/2007 Upstream (S) elevation.



Site no LAIAR-005-022 **Baylough Bridge; {Bella Bridge}**

County	Laois; Kildare	Townland	Inchacooly; Co Kildare	Town	
Planning	Laois CC; Kildare CC	Discovery map	55	Six-inch map	5

Summary A triple-arch masonry road bridge of 18th/early 19th century date over the River Barrow on the boundary with Co Kildare.

History Bella Bridge is cited on the 1838 OS map. A bridge is shown but not cited on the 1888 edition. Baylough Bridge is captioned on the 1907 map. It is of 18th/early 19th century date.

Component LAIAR-005-022 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	260614 212379		+/- 10m		
Survey date	23/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge

Description A triple-span arched masonry bridge carries a public road over the River Barrow at the boundary with Co Kildare. It is of random rubble construction throughout. There are angled cutwaters to the up- and downstream ends of the cutwaters. These rise to arch spring level and are of coursed masonry blocks. Concrete footings have been added to the abutments, piers and cutwaters. The arches are of segmental profile and rise to the centre of the bridge. Their voussoirs are of dressed masonry. From the left to right bank, they span 7.65m, 9.24m and 7.67m. The parapets are 6.38m apart. They are coped with concrete on the Co Laois section of the bridge, and with random rubble on the Kildare side.

Interest Architectural

Evaluation This is a good example of an unadorned later 18th/early 19th century rubble masonry road bridge over a significant river. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.

Rating	Regional	Protection		Action	RPS
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Associated with

Other dbase LA Bridge C012

References

Duffy, J. Barrow Bridges (Tullow: John Duffy, 2007). Page(s) 102

LAIAR-005-022_01 FWH 23/04/2007 Upstream (W) elevation.



LAIAR-005-022_02 FWH 23/04/2007 Downstream (E) elevation.



LAIAR-005-022_03 FWH 23/04/2007 Pier detailing on downstream side.



Site no LAIAR-005-024

County	Laois	Townland	Ballymorris	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	5

Summary Slightly skew arched masonry road bridge over Dublin-Cork railway line, opened by Great Southern & Western Railway Co in 1847.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway was opened in 1847. An uncaptioned bridge is marked on the 1888 and 1907 OS maps.

Component LAIAR-005-024 1

Type	Bridge (road/rail)	Function	Railway		
Category	Transport	Context	Infrastructure		
Irish Grid	254103 210630		+/- 10m		
Survey date	11/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge

Description A slightly skewed single-span masonry arch bridge carries a road over the double-track Dublin-Cork railway. The abutments are of regularly-coursed rock-faced masonry blocks, with margined quoins. The buttresses to each end are of identical construction. The arch is of semi-elliptical profile and spans 9.14m (measured orthogonally; 9.63m on the skew). It has rusticated voussoirs (with margined edges to each block) which step into the spandrels. Its soffit is of masonry blocks, laid orthogonally. A string course runs around the quoins and through the arch at spring level. The spandrels are of roughly-dressed and squared rubble set in regular courses. A string course runs over the crown of the arch and across the buttresses and wing walls. The parapets, which are 6.67m apart, are of dressed and squared rubble, randomly laid and coped with out-projecting dressed stone blocks. The wing walls are similar to the parapets. Signs on the abutment quoins note this bridge as Iarnród Éireann bridge no.120. The road approaches are dog-legged and ramped.

Interest Architectural; Historical

Evaluation This bridge is of architectural interest on account of the quality and detailing of the stonework. It is also of historical interest due to its association with the Great Southern & Western Railway Co. It is of local industrial heritage significance.

Rating	Local	Protection		Action	None
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Associated with LAIAR-005-023

Other dbase

References

LAIAR-005-024_01 FWH 11/04/2007 NE elevation.



Site no LAIAR-005-027

County Laois **Townland** Cooltedery **Town** Portarlinton

Planning Laois CC **Discovery map** 55 **Six-inch map** 5

Summary Twin-span road bridge over Dublin-Cork/Galway railway line, opened by Great Southern & Western Railway Co in 1847. The deck is a later 1900s reinforced-concrete replacement.

History This railway line was opened as far as Portlaoise by the Great Southern & Western Railway in 1847 (it reached Cork in 1849). The bridge is shown on the 1888 and 1907 OS maps. Its deck was replaced in the later 1900s.

Component LAIAR-005-027 1

Type Bridge (road/rail) **Function** Railway

Category Transport **Context** Infrastructure

Irish Grid 254731 210866 +/- 10m

Survey date 23/04/2007 **Surveyor** Fred Hamond

Remains Complete **Condition** Good **Current use** Road bridge

Description A twin-span reinforced-concrete girder bridge carries a public road over the Dublin-Cork railway immediately west of Portarlinton Station. The abutments, which are part of the original structure, are of coursed, rock-faced limestone blocks with tooled arrises. The pier is of squared limestone, laid randomly. The upper sections of the abutments and piers have been replaced with mass concrete. A dressed masonry string course runs along the line of the original arch springing on the main (north) span. The original spans were replaced with pre-cast reinforced-concrete beams and slab deck. The north span is 10.20m and the south one 5.60m. The parapets, which are 10.26m apart, are of concrete and terminate in mass concrete piers. Squared limestone rubble wing walls project at right angles on the north end. Road ramped up to both ends. Iarnród Éireann bridge no.119.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with LAIAR-005-023

Other dbase Mountmellick Canal F917

References

LAIAR-005-027_01 FWH 23/04/2007 East elevation.



Site no LAIAR-005-028 **Portarlinton Station**

County Laois **Townland** Cooltedery **Town** Portarlinton

Planning Laois CC **Discovery map** 55 **Six-inch map** 5

Summary Station opened by Great Southern & Western Railway Co in 1847 on Dublin-Cork line. Also changeover point for Athlone/Galway line (opened 1854). The complex, which was designed by Sancton Wood, encompasses the station building, engine shed, metal foot bridge, modern replacement foot bridge, metal water tank and signal box; also a Victorian pillar box. The lattice metal foot bridge dates from the 1880s and was manufactured by Edward Manisty (Dundalk). It is no longer in use.

History A foot bridge is shown on the 1907 OS map but is not on the 1888 edition. A photograph of 1890 also shows it, implying that it is was erected in the late 1880s.

Component LAIAR-005-028 8

Type Bridge (foot/rail) **Function** Railway

Category Transport **Context** Infrastructure

Irish Grid 254820 210880 +/- 10m

Survey date 23/04/2007 **Surveyor** Fred Hamond

Remains Complete **Condition** Good **Current use** Disused

Description A single-span metal lattice girder foot bridge over the double-track railway line at Portarlinton Station. The span comprises T-section top and bottom strings between which is lattice bracing. The girders are also braced to their outside faces with curved metal brackets from the underside of the deck. Metal plates are laid across the bottom strings of the girders to form the deck. At each end of the deck are cast-iron steps down to quarter-turn half-landings supported on cast-iron columns, from which more steps lead down to the platforms. 'E Manisty Dundalk' is cast on the uprights supporting the half-landing on the south (down) platform. The steps have round newels surmounted by acorn finials. The free span is 14.49m and the deck is 1.40m wide. This is Iarnród Éireann bridge no.118. No longer in use, having been superseded by a modern foot bridge to its east.

Interest Architectural; Group; Historical; Rarity; Setting; Technical

Evaluation Primarily of architectural, historical and technical interest as a typical, but now rare, example of a Great Southern & Western station lattice girder foot bridge. Enhances interest of station and also has group value with rest of complex. It is of regional industrial heritage significance and merits its present inclusion in Record of Protected Structures (LA 143A).

Rating Regional **Protection** RPS LA 143 (station); RPS LA 143A (foot bridge); RPS LA 143B (letter box) **Action** None (in RPS)

Associated with LAIAR-005-023

Other dbase NIAH 12800502 (N); NIAH 12800503 (R); NIAH 12800504 (R); Mountmellick Canal F916

References

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- Mathews, R. Portarlinton: the Inside Story. Page(s) 94
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LAIAR-005-028_01 FWH 23/04/2007 Footbridge from west (up platform).



LAIAR-005-028_02 FWH 23/04/2007 Supporting column detail on down (south) platform.



Site no LAIAR-005-029

County	Laois	Townland	Tirhogar	Town
Planning	Laois CC	Discovery map	55	Six-inch map 5

Summary Single-span troughed metal girder rail bridge over accommodation track. This section of the Dublin-Cork railway was opened by the Great Southern & Western Railway Co in 1847.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway, to Portlaoise, was opened in 1847. An uncaptioned bridge is marked on the 1888 and 1907 OS maps.

Component LAIAR-005-029 1

Type	Bridge (rail/road)	Function	Railway		
Category	Transport	Context	Infrastructure		
Irish Grid	255742 210708		+/- 10m		
Survey date	23/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Rail bridge
Description	A single-span metal beam bridge carries the double-track Dublin-Cork railway over an accommodation track. The abutments are of squared masonry rubble, brought to courses. The 3.61m wide span is of longitudinal metal troughing and is undoubtedly a replacement. The parapets comprise single-bar steel railings which terminate in masonry piers. There are curved wing walls at each end, detailed as the abutments and coped with stone; they terminate in small piers. A modern metal staircase at the south-west end gives access to the track. Iarnród Éireann bridge no.395.				

Interest None

Evaluation No special industrial heritage significance.

Rating	Record only	Protection	Action	None
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Associated with LAIAR-005-023

Other dbase

References

LAIAR-005-029_01 FWH 23/04/2007 South elevation.



Site no LAIAR-005-030

County	Laois	Townland	Tirhogar	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	5

Summary Masonry arch road bridge over Dublin-Cork railway line, opened by Great Southern & Western Railway Co in 1847.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway, to Portlaoise, was opened in 1847. An uncaptioned bridge is marked on the 1888 and 1907 OS maps.

Component LAIAR-005-030 1

Type	Bridge (road/rail)	Function	Railway		
Category	Transport	Context	Infrastructure		
Irish Grid	256673 210437	+/- 10m			
Survey date	23/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A single-span masonry arch bridge carries a public road over the double-track Dublin-Cork railway. The abutments are of regularly-coursed rock-faced masonry blocks, with margined quoins. The arch is of semi-elliptical profile and spans 9.17m. It is embellished with vee-jointed rusticated voussoirs (with margined edges to each block) which radiate into the spandrels. Its soffit is of squared masonry blocks. A string course runs around the quoins and through the arch at spring level. The spandrels are a continuation of the voussoirs and are similarly detailed. A string course runs over the crown of the arch. The parapets, which are 6.46m apart, are of squared random rubble with out-projecting chamfered copings. Down-sloping wing walls, detailed as the parapets, project at right angles and terminate in small square piers. The deck has a slight curvature and the approaches are ramped and dog-legged. This is Iarnród Éireann bridge no.114.				

Interest Architectural; Historical

Evaluation This bridge is of architectural merit on account of the quality of its construction and detailing (notably the radial voussoirs/spandrels). It also has a historical association with the Great Southern & Western Railway. Overall, it is of local industrial heritage merit.

Rating	Local	Protection		Action	None
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Associated with LAIAR-005-023

Other dbase

References

LAIAR-005-030_01 FWH 23/04/2007 West elevation.



LAIAR-005-030_02 FWH 23/04/2007 Stonework detail on arch and spandrel.



Site no LAIAR-005-031

County	Laois	Townland	Tirhogar	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	5

Summary Masonry arch road bridge over Dublin-Cork railway line, opened by Great Southern & Western Railway Co in 1847.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway, to Portlaoise, was opened in 1847. An uncaptioned bridge is marked on the 1888 and 1907 OS maps.

Component LAIAR-005-031 1

Type	Bridge (road/rail)	Function	Railway		
Category	Transport	Context	Infrastructure		
Irish Grid	257206 210277	+/- 10m			
Survey date	23/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A single-span masonry arch bridge carries a public road over the double-track Dublin-Cork railway. The abutments are of regularly-coursed rock-faced masonry blocks, with margined quoins. There are roughly-dressed coursed masonry block buttresses to each side. The arch is of semi-elliptical profile and is embellished with vee-jointed rusticated voussoirs (with margined edges to each block) which radiate into the spandrels. Its soffit is of squared masonry blocks. A string course runs around the quoins and through the arch at spring level. The spandrels are a continuation of the voussoirs and are similarly detailed. A string course runs over the crown of the arch. The parapets, which are 3.72m apart, are of squared random rubble with out-projecting chamfered copings. The north end of the west parapet has been rebuilt. Wing walls, detailed as the parapets, curve around from the buttresses and terminate in piers which continue upwards to form terminal piers to the parapets. These piers are of roughly-dressed and coursed masonry blocks, with tooled quoins. The deck is humped and the approaches are ramped and dog-legged. This is Iarnród Éireann bridge no.113.				

Interest Architectural; Historical

Evaluation This bridge is of architectural merit on account of the quality of its construction, detailing (notably the radial voussoirs/spandrels) and completeness. It also has a historical association with the Great Southern & Western Railway. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.

Rating	Regional	Protection	Action	RPS
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Associated with LAIAR-005-023

Other dbase

References

LAIAR-005-031_01 FWH 23/04/2007 West elevation.



Site no LAIAR-005-033

County	Laois	Townland	Derrynafunshion	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	5

Summary Masonry arch road bridge over Dublin-Cork railway line, opened by Great Southern & Western Railway Co in 1847.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway, to Portlaoise, was opened in 1847. An uncaptioned bridge is marked on the 1888 and 1907 OS maps.

Component LAIAR-005-033 1

Type	Bridge (road/rail)	Function	Railway		
Category	Transport	Context	Infrastructure		
Irish Grid	258084 210178	+/- 10m			
Survey date	23/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A single-span masonry arch bridge carries a public road over the double-track Dublin-Cork railway. The abutments are of regularly-coursed rock-faced masonry blocks, with margined quoins. There are dressed and coursed masonry block buttresses to each side. The arch is of semi-elliptical profile and is embellished with vee-jointed rusticated voussoirs (with margined edges to each block) which radiate into the spandrels. Its soffit is of squared masonry blocks. A string course runs around the quoins and through the arch at spring level. The spandrels are a continuation of the voussoirs and are similarly detailed. A string course runs over the crown of the arch. The parapets, which are 3.77m apart, are of roughly dressed and squared random rubble with out-projecting chamfered copings. They terminate in piers which are upward continuations of the buttresses. Wing walls, detailed as the parapets, project at right angles. The approaches are ramped. This is Iarnród Éireann bridge no.112.				

Interest Architectural; Historical

Evaluation This bridge is of architectural merit on account of the quality of its construction and detailing (notably the radial voussoirs/spandrels). It also has a historical association with the Great Southern & Western Railway. Overall, it is of local industrial heritage merit.

Rating	Local	Protection		Action	None
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Associated with LAIAR-005-023

Other dbase

References

LAIAR-005-033_01 FWH 23/04/2007 West elevation.



Site no LAIAR-005-034

County	Laois	Townland	Kilmullen	Town
Planning	Laois CC	Discovery map	55	Six-inch map 5

Summary Site of accommodation bridge over Dublin-Cork railway line, opened by Great Southern & Western Railway Co in 1847.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway, to Portlaoise, was opened in 1847. An uncaptioned bridge is marked on the 1888 and 1907 OS maps. It was probably demolished in the relatively recent past in connection with the upgrading of the line.

Component LAIAR-005-034 1

Type	Bridge (road/rail)	Function	Railway		
Category	Transport	Context	Infrastructure		
Irish Grid	258996 210163		+/- 10m		
Survey date	23/04/2007	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	Site of accommodation bridge over the double-track Dublin-Cork railway. All traces of the abutments have been cleared.				

Interest None

Evaluation No industrial heritage significance.

Rating	Record only	Protection	Action	None
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Associated with LAIAR-005-023

Other dbase

References

Site no LAIAR-005-036

County	Laois	Townland	Kilmullen	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	5

Summary Masonry arch road bridge over Dublin-Cork railway line, opened by Great Southern & Western Railway Co in 1847. The parapets are largely replaced.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway, to Portlaoise, was opened in 1847. An uncaptioned bridge is marked on the 1888 and 1907 OS maps.

Component LAIAR-005-036 1

Type	Bridge (road/rail)	Function	Railway		
Category	Transport	Context	Infrastructure		
Irish Grid	259460 210159		+/- 10m		
Survey date	23/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge

Description A single-span masonry arch bridge carries a public road over the double-track Dublin-Cork railway. The abutments are of regularly-coursed rock-faced masonry blocks, with margined quoins. The arch is of semi-elliptical profile and spans 9.13m. It is embellished with vee-jointed rusticated voussoirs (with margined edges to each block) which radiate into the spandrels. Its soffit is of squared masonry blocks. A string course runs around the quoins and through the arch at spring level. The spandrels are a continuation of the voussoirs and are similarly detailed. A string course runs over the crown of the arch. The parapets, which are 6.25m apart, have mostly been replaced with concrete blockwork, with only vestiges of the original dressed and squared random rubble walls surviving (with out-projecting chamfered copings). Wing walls, detailed as the parapets, project at right angles. The approaches are doglegged and ramped, and the deck is curved. This is Iarnród Éireann bridge no.109.

Interest Historical

Evaluation Although this bridge has an historical association with the Great Southern Railway, its architectural character is diminished by the replacement concrete block parapet to such an extent that it is no longer of any particular industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with LAIAR-005-023

Other dbase

References

LAIAR-005-036_01 FWH 23/04/2007 West elevation.



LAIAR-005-036_02 FWH 23/04/2007 Warning signs on north approach to bridge.



Site no LAIAR-005-037

County	Laois	Townland	Clonanny	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	5

Summary Single-span troughed metal girder rail bridge over public road. This section of the Dublin-Cork railway was opened by the Great Southern & Western Railway Co in 1847.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway, to Portlaoise, was opened in 1847. An uncaptioned bridge is marked on the 1888 and 1907 OS maps.

Component LAIAR-005-037 1

Type	Bridge (rail/road)	Function	Railway		
Category	Transport	Context	Infrastructure		
Irish Grid	260012 210154		+/- 10m		
Survey date	23/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Rail bridge
Description	A single-span metal beam bridge carries the double-track Dublin-Cork railway over an accommodation track. The abutments, which are 9.45m deep, are of dressed and squared masonry rubble, laid randomly. The 3.61m wide span is of riveted longitudinal metal troughing. The parapets comprise two-bar steel railings; masonry terminal piers survive on the south side only. There are curved wing walls at each end, detailed as the abutments and coped with stone; they terminate in small piers.				

Interest None

Evaluation No special industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with LAIAR-005-023

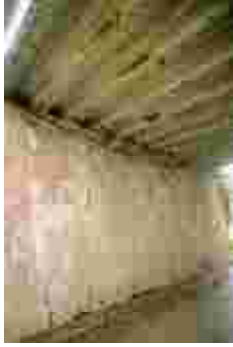
Other dbase

References

LAIAR-005-037_01 FWH 23/04/2007 South elevation.



LAIAR-005-037_02 FWH 23/04/2007 Underside of deck.



Site no LAIAR-005-040

County	Laois	Townland	Bracklone	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	5

Summary A culvert of c.1828 carries a drain under the Mountmellick Branch of the Grand Canal, here dewatered

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. This aqueduct is cited on the 1838 and 1888 OS maps, and shown on the 1907 edition.

Component LAIAR-005-040 1

Type	Bridge (canal/river)	Function	Inland waterway		
Category	Transport	Context	Infrastructure		
Irish Grid	255608 211713		+/- 10m		
Survey date	22/08/2006	Surveyor	Fred Hamond		
Remains	Complete	Condition	Fair	Current use	Culvert
Description	This culvert carries a drainage channel under the Mountmellick Canal, here dewatered. The arch is of segmental profile and c.1.2m wide. It and the spandrels are of dressed limestone blocks laid to courses. The wing walls are of similarly dressed stone, but laid randomly. No parapet. Drainage channel heavily silted but still functioning.				

Interest Historical

Evaluation Historical association with canal. Of local industrial heritage significance.

Rating	Local	Protection		Action	None
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Associated with LAIAR-005-007

Other dbase Mountmellick Canal F108

References

LAIAR-005-040_01 FWH 22/08/2006 South elevation of culvert.



Site no LAIAR-005-041

County Laois **Townland** Bracklone **Town**
Planning Laois CC **Discovery map** 55 **Six-inch map** 5

Summary Site of an culvert of c.1828 carrying a drain under the Mountmellick Branch of the Grand Canal.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. This aqueduct is cited on the 1838 and 1888 OS maps, and shown on the 1907 edition.

Component LAIAR-005-041 1

Type Bridge (canal/river) **Function** Inland waterway
Category Transport **Context** Infrastructure
Irish Grid 256152 211648 +/- 10m
Survey date 22/08/2006 **Surveyor** Fred Hamond
Remains No visible remains **Condition** N/A **Current use** N/A
Description Site of a culvert carrying a drainage channel under the Mountmellick Canal.

Interest None

Evaluation No industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with LAIAR-005-007

Other dbase Mountmellick Canal F107

References

Site no LAIAR-005-050

County	Laois	Townland	Cooltedery	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	5

Summary Single-span troughed metal rail bridge over accommodation track. This section of the Dublin-Cork railway was opened by the Great Southern & Western Railway Co in 1847. The bridge was widened on both sides in reinforced-concrete in the later 1900s.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway, to Portlaoise, was opened in 1847. An uncaptioned bridge is marked on the 1888 and 1907 OS maps. It was widened in reinforced-concrete in the later 1900s.

Component LAIAR-005-050 1

Type	Bridge (rail/road)	Function	Railway		
Category	Transport	Context	Infrastructure		
Irish Grid	255212 210863		+/- 10m		
Survey date	23/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Rail bridge
Description	A single-span metal beam bridge carries the double-track Dublin-Cork railway over an accommodation track. The abutments are of rock-faced masonry blocks, brought to courses; they are 9.40m deep. The 3.60m wide span is of longitudinal metal troughing and is a replacement. The parapet comprises a two-bar steel railing. There are curved wing walls at each end, detailed as the abutments. They are coped with concrete. The bridge has been widened by 3.40m on each side with the addition of reinforced-concrete slab decks. The ends of each deck rest on concrete beams, one end of which is supported on the original masonry abutment, and the other on a concrete column.				

Interest None

Evaluation No special industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with

Other dbase

References

LAIAR-005-050_01 FWH 23/04/2007 NE elevation.



LAIAR-005-050_02 FWH 23/04/2007 Underside of deck showing troughing and concrete extension.



Site no LAIAR-005-051

County	Laois	Townland	Cooltedery	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	5

Summary Single-span troughed metal girder rail bridge over accommodation track. This section of the Dublin-Cork railway was opened by the Great Southern & Western Railway Co in 1847.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway, to Portlaoise, was opened in 1847. An uncaptioned bridge is marked on the 1888 and 1907 OS maps.

Component LAIAR-005-051 1

Type	Bridge (rail/road)	Function	Railway		
Category	Transport	Context	Infrastructure		
Irish Grid	255533 210771		+/- 10m		
Survey date	23/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Rail bridge

Description A single-span metal beam bridge carries the double-track Dublin-Cork railway over an accommodation track. The abutments are of squared masonry rubble, brought to courses; they are 9.51m deep. The east abutment has been gunited. The 3.57m wide span is of longitudinal metal troughing and is a replacement. The parapets comprise two-bar steel railings; masonry terminal piers survive on the north side only. There are angled wing walls at each end, detailed as the abutments, but randomly laid and coped with stone; they terminate in small piers. The track is in the process of being realigned hereabouts, and it is possible that the passage under the line will be blocked to vehicular traffic.

Interest None

Evaluation No special industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with

Other dbase

References

LAIAR-005-051_01 FWH 23/04/2007 South elevation.



Site no	LAIAR-005-052	New Channel Bridge
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County	Laois	Townland	Cooltedery	Town	Portarlington
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Planning	Laois CC	Discovery map	55	Six-inch map	5
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Summary Presumed site of 17th century road bridge over moat on southern approach to Portarlington.

History Queen Street crossed the town moat at this point in the 1600s.

Component LAIAR-005-052 1

Type	Bridge (road/river)	Function	Road & pedestrian
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Category	Transport	Context	Infrastructure
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Irish Grid	254266 212507 +/- 10m
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Survey date	11/04/2007	Surveyor	Fred Hamond
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Remains	No visible remains	Condition	N/A	Current use	N/A
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Description No visible traces of bridge now apparent.

Interest Archaeological

Evaluation Although this site is in the Record of Monuments & Places (LA005-031009-), it is of no special industrial heritage significance in respect of its upstanding remains.

Rating	Record only	Protection	RMP LA005-031009-	Action	None
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Associated with

Other dbase SMR LA005-031009-

References

Archaeological Survey of Ireland: Sites & Monuments Record print-out.	Page(s)
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Site no LAIAR-006-005

County Bridge

County	Laois; Offaly	Townland	Ballynahown; Co Offaly	Town	
Planning	Laois CC; Offaly CC	Discovery map	54	Six-inch map	6

Summary An 18th/early 19th century masonry road bridge over County River at county boundary. Extensively repaired and widened c. 1921 after being damaged by flood c.1918.

History County Bridge is cited on the 1838, 1888 and 1910 OS maps. According to John Feehan, it was damaged by a flood c.1918 and repaired/widened c.1921.

Component LAIAR-006-005 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	224067 209985		+/- 10m		
Survey date	10/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge

Description An arched masonry bridge carries the Cadamstown-Clonaslee road over the County River which here runs in a gorge. Of random rubble construction throughout. Masonry on upstream portion of abutments has been brought to courses. The arch is of semicircular profile and spans c.3.9m. Part of the abutments and soffit have been gunited. A break in the soffit suggests a widening - probably c.1921 when the bridge was repaired. There are also concrete repairs to the soffit. Voussoirs are of finely dressed sandstone blocks. Parapets are 9.53m apart and coped with concrete. Ivy overgrowth precluded any inspection of the upstream (south-east) face and made photography impossible.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase LA Bridge R001; OFIAR-032-003

References

Feehan, J. The Landscape of Slieve Bloom: a Study of its Natural and Human Heritage (Dublin: Blackwater Press, 1979).

Page(s) 155

LAIAR-006-005_01 FWH 27/08/2004 Downstream (north-west) elevation.



Site no LAIAR-006-006

County Laois; Offaly **Townland** Ballynahown; Co Offaly **Town**
Planning Laois CC; Offaly CC **Discovery map** 54 **Six-inch map** 6

Summary An 18th/early 19th century masonry road bridge over County River.

History A bridge is shown on the 1838, 1884 and 1910 OS maps.

Component LAIAR-006-006 1

Type Bridge (road/river) **Function** Road & pedestrian
Category Transport **Context** Infrastructure
Irish Grid 224397 209563 +/- 10m
Survey date 10/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge
Description An arched masonry bridge carries a minor road over the County River on county boundary. Of random rubble masonry throughout. The arch is of semicircular profile and its voussoir is of dressed sandstone. It spans 6.22m and its abutments are 8.76m deep. The abutments and spandrels have been gunited and there are tie bars through the latter. The parapets have been replaced by metal crash barriers.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase

References

LAIAR-006-006_01 FWH 10/04/2007 Upstream (E) elevation.



Site no **LAIAR-007-001** **Bloody Bridge**

County Laois **Townland** Garroon or Summergrove;
Mullaghanard or Derreen **Town**

Planning Laois CC **Discovery map** 54 **Six-inch map** 7

Summary A highly skewed 18th/early 19th century masonry culvert carrying a tributary of the Murglash River under a road.

History A bridge is shown on the 1839, 1888 and 1907 OS maps. It is explicitly captioned as Bloody Bridge on the 1907 map, supposedly after a battle fought nearby during the Cromwellian wars.

Component **LAIAR-007-001 1**

Type Bridge (road/river) Function Road & pedestrian

Category Transport Context Infrastructure

Irish Grid 241672 207966 +/- 10m

Survey date 10/04/2007 Surveyor Fred Hamond

Remains Complete Condition Good Current use Road bridge

Description A highly-skewed masonry arch culvert carries a tributary of Murglash River under a minor road. It is of random rubble construction throughout. The arch is of segmental profile and measures 1.57m orthogonally/ 1.89m on the skew. The overgrown parapets are c.7.5m apart, as measured on the skew.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase LA Bridge C302

References

Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002). Page(s) 133

LAIAR-007-001_01 FWH 10/04/2007 Upstream (W) elevation.



Site no **LAIAR-007-004** **Owenass Bridge**

County Laois **Townland** Derrycloney **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 7

Summary An 18th/early 19th century masonry road bridge over Owenass River.

History Owenass Bridge is captioned on the 1839, 1888 and 1907 OS maps.

Component **LAIAR-007-004 1**

Type Bridge (road/river) **Function** Road & pedestrian
Category Transport **Context** Infrastructure
Irish Grid 244127 206558 +/- 10m
Survey date 10/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge

Description An arched masonry bridge carries a minor road over the Owenass River. It is of random rubble construction throughout. The arch is of segmental profile and has dressed voussoirs; it spans 9.97m. The downstream (NE) spandrel and parapet have been rebuilt with concrete blocks. The rebuilt parapet is faced with random rubble to its road face and is 10.05m from its counterpart on the upstream side. The parapets are coped with stone set on edge. The middle section of the soffit is plastered with cement. However, there is no evidence of any widening (which would probably be apparent as differences in the up and downstream elevations); there may have been a pipe across the inside of the arch, since removed. There are projecting stones on the right-bank abutment which formerly supported the arch falsework. The deck is humped (this is particularly pronounced when driving south).

Interest Architectural

Evaluation This substantial bridge is a good example of its type and very similar in size and design to one further upstream at Barkmill (LAIAR-007-006). It is of local industrial heritage significance but not sufficiently special to justify inclusion in the Record of Protected Structures. The bridge's curved deck and high volume of traffic over it pose a threat to its long-term survival.

Rating Local **Protection** **Action** None

Associated with

Other dbase LA Bridge C032

References

Eason Photographic Collection, Laois County Library (Local Studies Dept), Page(s)
Portlaoise.
Eason Photographic Collection, Laois County Library (Local Studies Dept), Page(s) E2703
Portlaoise.

LAIAR-007-004_01 FWH 10/04/2007 Downstream (NE) elevation.



LAIAR-007-004_02 FWH 10/04/2007 Falsework projections on right-bank abutment.



Site no LAIAR-007-005

Cathole Bridge

County	Laois	Townland	Ballyfin; Ballyhuppahane	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	7

Summary An 18th/early 19th century twin-arch masonry road bridge over Owenass River.

History Cathole Bridge is captioned on the 1839, 1888 and 1907 OS maps.

Component LAIAR-007-005 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	237681 204903		+/- 10m		
Survey date	10/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge

Description A twin-arch masonry bridge carries a minor road over the Owenass River. It is of random rubble construction throughout. There is a triangular cutwater on the upstream (W) face, rising to arch spring level. The arches are of segmental span and have finely-dressed voussoirs. The left-bank one is slightly lower and narrower than the other. The former spans 3.29m and the latter 4.93m. The parapets are coped with concrete and are 6.21m apart. The bed of the river has been concreted. The left arch is now inactive as it is blocked by a structure associated with the Laois County Council water abstraction point just upstream.

Interest Architectural

Evaluation A good example of a twin-arch masonry bridge beside a public amenity (Cathole Falls). Of local industrial heritage significance but not sufficiently special to warrant inclusion in Record of Protected Structures.

Rating	Local	Protection		Action	None
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Associated with

Other dbase LA Bridge C065

References

Eason Photographic Collection, Laois County Library (Local Studies Dept), Portlaoise.	Page(s)	E2700
Eason Photographic Collection, Laois County Library (Local Studies Dept), Portlaoise.	Page(s)	
Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002).	Page(s)	69

LAIAR-007-005_01 FWH 10/04/2007 Upstream (W) elevation.



LAIAR-007-005_02 FWH 10/04/2007 Downstream (E) elevation.



Site no LAIAR-007-006

County Laois **Townland** Barkmill; Skerry **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 7

Summary An 18th/early 19th century masonry road bridge over Owenass River.

History A bridge is shown on the 1839, 1888 and 1907 OS maps.

Component LAIAR-007-006 1

Type Bridge (road/river) **Function** Road & pedestrian
Category Transport **Context** Infrastructure
Irish Grid 240266 204750 +/- 10m
Survey date 10/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge

Description An arched masonry bridge carries a minor road over the Owenass River. It is of rubble stone construction throughout. The arch is of segmental profile and has finely dressed voussoirs; its spans 9.80m. The crown of the arch is 4.45m above the bed of the river. At the downstream end of the right-bank abutment is a square recess, possibly for a falsework beam. The parapets are coped with concrete. The bed of the river has been concreted and the right bank has been stepped with concrete on the downstream side.

Interest Architectural

Evaluation This substantial bridge is a good example of its type and very similar in size and design to Owenass Bridge, near Mountmellick (LAIAR-007-004). Although of local industrial heritage significance, it is not sufficiently special to justify inclusion in the Record of Protected Structures.

Rating Local **Protection** **Action** None

Associated with

Other dbase LA Bridge C061

References

LAIAR-007-006_01 FWH 10/04/2007 Upstream (W) elevation. Note soffit.



LAIAR-007-006_02 FWH 10/04/2007 Downstream (E) elevation.



LAIAR-007-006_03 FWH 10/04/2007 Downstream (E) elevation.



Site no **LAIAR-007-008**

County	Laois	Townland	Cloncanon Lower; Omoresforest	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	7

Summary A later 19th/early 20th century single-span road bridge over Owenass River, replacing earlier previous stones. Its deck was replaced with a metal beam and concrete slab in the later 1900s.

History Stepping stones are captioned on the 1839 and 1888 OS maps. A bridge is first shown on the 1907 edition. It appears to have been a beam bridge, probably a metal girder, the deck of which was replaced in reinforced-concrete in the later 1900s.

Component **LAIAR-007-008 1**

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	243103 205648	+/- 10m			
Survey date	10/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A single-span metal beam and concrete bridge carries a minor road over the Owenass River. The deck comprises a 3.27m wide reinforced-concrete slab over two RSJs (51cm x 18cm section at 2.08m centres) and spans 9.10m. It rests on concrete pillow blocks on random rubble abutments belonging to the previous bridge. The parapets comprise two tubular metal railings.				

Interest None

Evaluation No special industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with

Other dbase LA Bridge C303

References

LAIAR-007-008_01 FWH 10/04/2007 Upstream (SW) elevation. Note beams and slab deck.



LAIAR-007-008_02 FWH 10/04/2007 Downstream (NE) elevation.



Site no **LAIAR-007-009** **Blackwater Bridge**

County Laois **Townland** Derrycloney; Omoresforest **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 7

Summary An 18th/early 19th century skew-arched masonry road bridge over River Blackwater.

History Blackwater Bridge is captioned on the 1839, 1888 and 1907 OS maps.

Component **LAIAR-007-009 1**

Type Bridge (road/river) Function Road & pedestrian
Category Transport Context Infrastructure
Irish Grid 243582 205679 +/- 10m
Survey date 10/04/2007 Surveyor Fred Hamond
Remains Complete Condition Good Current use Road bridge
Description A skew arched masonry bridge carries a minor road over the Blackwater River. It is of random rubble construction throughout. The arch is of semicircular profile and has dressed voussoirs; it spans 4.58m (measured orthogonally)/ 4.74m on the skew. The parapets are 6.15m apart.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase LA Bridge C036

References

LAIAR-007-009_01 FWH 10/04/2007 Upstream (SE) elevation.



Site no LAIAR-007-009 **Blackwater Bridge**

County Laois **Townland** Derrycloney; Omoresforest **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 7

Summary An 18th/early 19th century skew-arched masonry road bridge over River Blackwater.

History Blackwater Bridge is captioned on the 1839, 1888 and 1907 OS maps.

Component LAIAR-007-009 1

Type Bridge (road/river) Function Road & pedestrian
Category Transport Context Infrastructure
Irish Grid 243582 205679 +/- 10m
Survey date 10/04/2007 Surveyor Fred Hamond
Remains Complete Condition Good Current use Road bridge
Description A skew arched masonry bridge carries a minor road over the Blackwater River. It is of random rubble construction throughout. The arch is of semicircular profile and has dressed voussoirs; it spans 4.58m (measured orthogonally)/ 4.74m on the skew. The parapets are 6.15m apart.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase LA Bridge C036

References

LAIAR-007-009_01 FWH 10/04/2007 Upstream (SE) elevation.



Site no LAIAR-007-010

Lady's Bridge

County Laois **Townland** Deerpark (Maryborough West By) **Town**

Planning Laois CC **Discovery map** 54 **Six-inch map** 7

Summary An 18th/early 19th century masonry road bridge over Owennahallia River.

History Lady's Bridge is captioned on the 1839, 1888 and 1907 OS maps.

Component LAIAR-007-010 1

Type Bridge (road/river) **Function** Road & pedestrian
Category Transport **Context** Infrastructure
Irish Grid 238159 202841 +/- 10m
Survey date 10/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge
Description An arched masonry bridge carries a minor road over the Owenahallia River. It is of rubble stone construction throughout. The arch is of semicircular profile and has dressed voussoirs, the upstream one of which projects slightly from the face of the bridge; its spans 4.33m. The parapets are 3.77m apart.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase LA Bridge C059

References

Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002). Page(s) 120

LAIAR-007-010_01 FWH 10/04/2007 Upstream (SW) elevation.



Site no LAIAR-008-005

County	Laois	Townland	Derrycloney	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	8

Summary A skew single-span metal girder railway bridge over Owenass River. On Mountmellick Branch of Great Southern & Western Railway, opened 1885. Now derelict.

History This bridge was built by the Central Ireland Railway Company and opened in 1885. It is shown on the 1888 and 1907 OS maps. It served as the Mountmellick Branch of the Great Southern & Western Railway. The line was closed to passenger services in 1947 and to goods traffic in 1963.

Component LAIAR-008-005 1

Type	Bridge (rail/river)	Function	Railway		
Category	Transport	Context	Infrastructure		
Irish Grid	244304 206690		+/- 10m		
Survey date	10/04/2007	Surveyor	Fred Hamond		
Remains	Substantial remains	Condition	Fair	Current use	Disused
Description	A highly-skewed metal girder bridge formerly carried the Portlaoise-Mountmellick railway over the Owenass River. The abutments are of roughly dressed and squared random rubble, now heavily overgrown with ivy. The span comprises two riveted metal plate girders of 94cm x 38cm section at 3.81m centres. Between their bottom flanges run 11 transverse I-beams of 30cm x 20cm section at 1.22m centres. On top of the latter are set two timber baulks of 30cm x 12cm section at 1.67m centres (5ft 6in standard railway gauge). The actual tracks were affixed to the tops of the beams but are long removed. The sides of the principal girders continue as squared rubble walls with concrete copings.				

Interest Architectural; Historical; Rarity

Evaluation This bridge is of architectural interest as a rare example (in a Laois context) of a late 19th century girder span. It is also of historical interest as a reminder of the former line of the railway between Mountmellick and Portlaoise. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.

Rating	Regional	Protection		Action	RPS
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Associated with LAIAR-008-001

Other dbase

References

LAIAR-008-005_01 FWH 10/04/2007 Downstream (NE) elevation.



LAIAR-008-005_02 FWH 10/04/2007 Looking north across deck.



LAIAR-008-005_03 FWH 10/04/2007 Underside of span from downstream (NE) side.



LAIAR-008-005_04 FWH 10/04/2007 Parapet terminal pier detail on downstream left-bank.



Site no LAIAR-008-007

County	Laois	Townland	Derrycloney	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	8

Summary A masonry road bridge over the Mountmellick Branch of Great Southern & Western Railway, opened in 1885. The bridge is now used as a footpath and the railway line is now a road.

History This bridge was built by the Central Ireland Railway Company and opened in 1885. It is shown on the 1888 and 1907 OS maps. It served as the Mountmellick Branch of the Great Southern & Western Railway. The line was closed to passenger services in 1947 and to goods traffic in 1963.

Component LAIAR-008-007 1

Type	Bridge (road/rail)	Function	Railway		
Category	Transport	Context	Infrastructure		
Irish Grid	244298 205889		+/- 10m		
Survey date	10/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Foot bridge
Description	An arched masonry bridge formerly carrying a road over a railway line. The latter is now a minor road and the road has become a recreational footpath. The abutments are of roughly-faced stone blocks, laid randomly. The arch is of semi-elliptical profile and spans 8.51m (28ft 0in). Its soffit is of dressed masonry blocks. The voussoirs are rusticated and have tooled edges around the arch ring. The spandrels are identical to the abutments, but brought to courses. The parapets are a continuation of the spandrels on their outside faces, but of squared random rubble on their inside faces; they are coped with concrete and are 7.62m apart. Wing walls project at right-angles; they are also of squared random rubble and are coped with concrete. The approach roads are ramped and doglegged.				

Interest Architectural; Historical; Setting

Evaluation Although a utilitarian structure, this bridge is remarkably well detailed. It is also a historical reminder of the former line of the railway between Mountmellick and Portlaoise and a prominent roadside feature on the southern approach to the town. It is of regional industrial heritage significance and merits its current inclusion in the Record of Protected Structures (LA 702).

Rating	Regional	Protection	RPS LA 702	Action	None (in RPS)
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Associated with LAIAR-008-001

Other dbase NIAH 12800838 (R)

References

Craig, M. and Garner, W. Second Report on Areas and Sites of Historical Interest in County Laois (Dublin: An Foras Forbartha, 1976).	Page(s)	90
Survey of the Architectural Heritage of County Laois (Dublin: NIAH, 2002).	Page(s)	

LAIAR-008-007_01 FWH 10/04/2007 North elevation.



LAIAR-008-007_02 FWH 10/04/2007 South elevation.



Site no LAIAR-008-009 **Pound Bridge**

County Laois **Townland** Townparks (Tinnahinch By) **Town** Mountmellick

Planning Laois CC **Discovery map** 54 **Six-inch map** 8

Summary An early 19th century road bridge over a tributary of River Barrow. The metal beam and concrete jack arch span is a 20th century replacement.

History A bridge is shown on the 1839, 1888 and 1907 OS maps. It is captioned as Pound Bridge on the 1907 edition. The original beam deck, which may have been of timber, appears to have been replaced with the present one in the 1900s.

Component LAIAR-008-009 1

Type Bridge (road/river) **Function** Road & pedestrian

Category Transport **Context** Infrastructure

Irish Grid 244433 207823 +/- 10m

Survey date 11/04/2007 **Surveyor** Fred Hamond

Remains Complete **Condition** Good **Current use** Road bridge

Description A single-span metal beam and concrete bridge carries a public road over a tributary of the River Barrow. The abutments are of random rubble. The span measures 4.55m and comprises nine RSJs (25cm x 12.5cm cross section) between which are concrete jack arches. The arches' corrugated metal formwork is still largely in place. The parapets comprise three-bar tubular steel railings. They terminate in squared masonry block piers embellished with tooled margins and shallow triangular caps. A footpath runs along each side of the 9.80m wide deck. Pipes are carried underneath the deck and through the abutments. The latter have concrete footings.

Interest Architectural

Evaluation The combination of 19th century masonry and 20th century beams and concrete jack arches are of sufficient architectural interest to make this bridge of local industrial heritage significance.

Rating Local **Protection** **Action** None

Associated with

Other dbase

References

LAIAR-008-009_01 FWH 11/04/2007 Downstream (NE) elevation.



LAIAR-008-009_02 FWH 10/04/2007 Underside of deck showing corrugated iron formwork to concrete jack arches.



Site no LAIAR-008-012 **Irishtown Bridge; {Steam Mill Bridge}**

County Laois **Townland** Ballycullenbeg; Townparks (Tinnahinch By) **Town** Mountmellick**Planning** Laois CC **Discovery map** 54 **Six-inch map** 8

Summary Site of an arched masonry road bridge over Owenass River, erected by Samuel Pim in 1837. Replaced by Laois CC in 1980 with a skewed reinforced-concrete beam and slab bridge.**History** An uncaptioned bridge is shown on the 1839, 1888 and 1907 OS maps. The present concrete bridge dates from 1990 and is a replacement of a single-span masonry arch erected in 1837. A commemorative plaque on the latter's downstream parapet read: "Erected by/ Samuel Pim/ Anno Domini 1837/ John Dunne builder". Now the responsibility of the National Roads Authority.

Component LAIAR-008-012 1**Type** Bridge (road/river) **Function** Road & pedestrian**Category** Transport **Context** Infrastructure**Irish Grid** 244990 207295 +/- 10m**Survey date** 11/04/2007 **Surveyor** Fred Hamond**Remains** No visible remains **Condition** N/A **Current use** N/A**Description** Site of an arched masonry bridge carrying public road over Owenass River at SW end of Mountmellick. It had a skew arch of segmental profile. A commemorative plaque on the river face of its downstream parapet noted that it was erected by Samuel Pim in 1837, the contractor being John Dunne. It was demolished when superseded by a new bridge immediately downstream in 1990. The whereabouts of its plaque is now unknown.**Component LAIAR-008-012 2****Type** Bridge (road/river) **Function** Road & pedestrian**Category** Transport **Context** Infrastructure**Irish Grid** 244999 207298 +/- 10m**Survey date** 11/04/2007 **Surveyor** Fred Hamond**Remains** Complete **Condition** Excellent **Current use** Road bridge**Description** A skew single-span reinforced-concrete bridge carries the N80 over the Owenass River at the SW end of Mountmellick. The abutments are of mass concrete. The span comprises a reinforced-concrete slab deck over pre-cast reinforced-concrete beams. It measures 9.69m at right angles between the abutments (11.49m on the skew). The parapets comprise vertical metal railings terminating in 'rusticated' concrete piers coped with concrete. The railings are 12.37m apart (measured orthogonally).

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase NRA Bridge LS-N80-009

References

Crawford, A. and Scott, M. Mountmellick Pictorial Memories.	Page(s) 48
Crawford, A. and Scott, M. Mountmellick Pictorial Memories.	Page(s) 46
Crawford, A. and Scott, M. Mountmellick Pictorial Memories.	Page(s) 45
Eason Photographic Collection, Laois County Library (Local Studies Dept), Portlaoise.	Page(s)
Mountmellick Bridge: Eirspan Inspection Report (National Roads Authority, 2002).	Page(s)
Scott, M. Mountmellick Pictorial Memories: Book 2 (1998).	Page(s) 10
Scott, M. Mountmellick Pictorial Memories: Book 3 (1999).	Page(s) 2

LAIAR-008-012_01 FWH 10/04/2007 Downstream (E) elevation.



LAIAR-008-012_02 FWH 10/04/2007 Detail on downstream left-bank pier.



Site no	LAIAR-008-016	Convent Bridge; {Big Bridge}
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County	Laois	Townland	Ballycullenbeg; Townparks (Tinnahinch By)	Town	Mountmellick
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Planning	Laois CC	Discovery map	54	Six-inch map	8
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Summary An 18th/early 19th century triple-arch masonry road bridge over Owenass River. A triple-span reinforced-concrete section was added to its downstream end during the later 1900s.

History A bridge is shown on the 1839, 1888 and 1907 OS maps. It is captioned as Big Bridge on the 1839 edition. The NIAH dates it to c.1809. It was widened on its downstream side in concrete during the later 1900s (according to the NIAH, c.1980). Now known as Convent Bridge after nearby convent.

Component LAIAR-008-016 1

Type	Bridge (road/river)	Function	Road & pedestrian
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Category	Transport	Context	Infrastructure
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Irish Grid	245507 207371	+/- 10m
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Survey date	11/04/2007	Surveyor	Fred Hamond
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Remains	Complete	Condition	Good	Current use	Road bridge
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Description A triple-span bridge carries the main road over the Owenass River. The upstream (SW) side is of masonry. The abutments and piers are of dressed stone blocks laid to courses. The piers have triangular cutwaters rising to arch spring level. The arches are of segmental profile and have dressed stone voussoirs with slightly projecting keystones. The widths of the spans from the left to right bank are: 4.22m, 4.82m and 4.23m. Their abutments and piers are c.7.98m deep. A string course runs through the arches at spring level and around the tops of the cutwaters. The spandrels and parapets are of random rubble. The parapet is rendered with cement to its road face and is coped with dressed stone blocks. It terminates in round dressed stone piers set on square bases. A pipe is carried across the tops of the cutwaters on the upstream side and the stone abutments and piers have concrete footings. The bridge has been widened by approx. 3.93m on its downstream (NE) side and the original parapet on this side has been removed. The new section comprises three reinforced-concrete slabs on concrete abutments and piers. From the left to right bank, they span 4.23m, 4.87m and 4.19m. There are no cutwaters to the piers. The downstream parapet comprises a vertical steel railing. There is also a footpath along this side.

Interest Architectural; Setting

Evaluation This is a well executed Grand Jury presentment bridge at the east end of Mountmellick. Its original character is clearly evident on its upstream face, but has been obscured on the other side by the concrete addition. The pipes across its up side diminish its original character. Despite these later alterations, sufficient of the original structure survives to make it of regional industrial heritage significance. It merits its current inclusion in the Record of Protected Structures (LA 701).

Rating	Regional	Protection	RPS LA 701	Action	None (in RPS)
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Associated with

Other dbase LA Bridge R012; NIAH 12900359 (R)

References

Crawford, A. and Scott, M. Mountmellick Pictorial Memories. Page(s) 40

Scott, M. Mountmellick Pictorial Memories: Book 2 (1998). Page(s) 11

Survey of the Architectural Heritage of County Laois (Dublin: NIAH, 2002). Page(s)

LAIAR-008-016_01 FWH 10/04/2007 Upstream (SW) elevation.



LAIAR-008-016_02 FWH 10/04/2007 Upstream elevation from left (W) bank. Note terminal pier at left.



LAIAR-008-016_03 FWH 10/04/2007 Downstream (NE) elevation.



Site no LAIAR-008-021

County	Laois	Townland	Strahard	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	8

Summary Site of aqueduct of c.1828 carrying Mountmellick Branch of Grand Canal over tributary of Triogue River.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. This aqueduct is captioned on the 1839, 1888 and 1907 OS maps.

Component LAIAR-008-021 1

Type	Bridge (canal/river)	Function	Inland waterway		
Category	Transport	Context	Infrastructure		
Irish Grid	246435 207738		+/- 10m		
Survey date	14/06/2006	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	The canal banks have been demolished and its bed infilled. No visible traces of the former aqueduct which carried the Mountmellick Canal over a tributary of the Triogue River.				

Interest None

Evaluation No industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with LAIAR-008-031

Other dbase Mountmellick Canal F311

References

Site no **LAIAR-008-022** **Debicot Bridge**

County Laois **Townland** Strahard **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 8

Summary A masonry arch accommodation bridge of c.1828 over the defunct Mountmellick Branch of Grand Canal. Now by-passed.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. Debicot Bridge is captioned on the 1839, 1888 and 1907 OS maps.

Component **LAIAR-008-022 1**

Type Bridge (road/canal) **Function** Inland waterway
Category Transport **Context** Infrastructure
Irish Grid 246876 207714 +/- 10m
Survey date 14/06/2006 **Surveyor** Fred Hamond
Remains Complete **Condition** Fair **Current use** Disused
Description A masonry arch bridge formerly carrying an accommodation road over a now-infilled section of the Mountmellick Branch of the Grand Canal. Of randomly coursed squared limestone rubble. The arch is of semicircular profile and spans c.6.74m. Its soffit is of squared rubble and the voussoirs and quoins are of finely dressed stone. A towpath runs through the south side (now buried along with the bed of the canal). Both faces of the bridge curve outwards to terminate in out-projecting piers embellished with finely dressed string course which continues around outside base of parapets. The inside faces of the parapets are of random rubble and are spaced at 3.46m; they are coped with finely dressed limestone blocks. Humped deck. No datestones. The approach roads have been removed at both ends and the bridge is now by-passed to east by a track across the infilled bed. Heavy ivy overgrowth.

Interest Architectural; Historical

Evaluation Of architectural and historical merit. Of regional industrial heritage significance. Merits inclusion in Record of Protected Structures.

Rating Regional **Protection** **Action** RPS

Associated with LAIAR-008-031

Other dbase IWAI MMC 200; Mountmellick Canal F310

References

LAIAR-008-022_01 FWH 14/06/2006 Debicot Bridge, from south-west.



LAIAR-008-022_02 FWH 14/06/2006 Debicot Bridge, from north-east.



LAIAR-008-022_03 FWH 14/06/2006 Debicot Bridge: north-east terminal pier detail.



Site no	LAIAR-008-023	Mountmellick Aqueduct
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County	Laois	Townland	Debicot	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	8

Summary A triple-span masonry aqueduct of c.1828 carries the defunct Mountmellick Branch of Grand Canal over the Triogue River.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. This aqueduct is captioned on the 1839, 1888 and 1907 OS maps.

Component LAIAR-008-023 1

Type	Bridge (canal/river)	Function	Inland waterway		
Category	Transport	Context	Infrastructure		
Irish Grid	247563 207520		+/- 10m		
Survey date	14/06/2006	Surveyor	Fred Hamond		
Remains	Complete	Condition	Fair	Current use	Disused
Description	A triple-arch bridge carries the defunct Mountmellick Branch of Grand Canal over the Triogue River. Ashlar limestone abutments and piers. Equi-sized spans (each about 2.5-3m across) are of segmental profile and embellished with vee-jointed voussoirs (not limestone). Rounded cutwaters at both ends, embellished with finely dressed limestone string courses. Random rubble spandrels. Ashlar string course over crowns, surmounted by ashlar limestone parapet. Approximately 20m deep. Heavy ivy overgrown. Tree roots now dislodging facing stones.				

Interest Architectural; Historical; Setting

Evaluation Of architectural and historical merit. Also enhances riverscape hereabouts. Of regional industrial heritage significance. Current inclusion in Record of Protected Structures merited (LA 531).

Rating	Regional	Protection	RPS LA 531	Action	None (in RPS)
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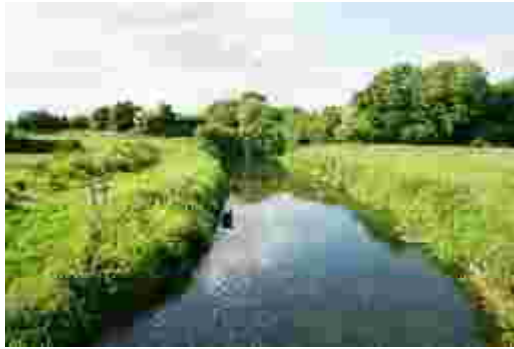
Associated with LAIAR-008-031

Other dbase IWAI MMC 190; NIAH 12800839 (R) ; Mountmellick Canal F309

References

Survey of the Architectural Heritage of County Laois (Dublin: NIAH, 2002). Page(s)

LAIAR-008-023_01 FWH 14/06/2006 Mountmellick Aqueduct from south.



LAIAR-008-023_02 FWH 14/06/2006 Mountmellick Aqueduct from upstream (south) side.



LAIAR-008-023_03 FWH 14/06/2006 Mountmellick Aqueduct from downstream (north) side.



LAIAR-008-023_04 FWH 14/06/2006 Looking east across Mountmellick Aqueduct.



Site no LAIAR-008-024

Triogue Bridge

County Laois **Townland** Debicot; Kilmainham **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 8

Summary A twin-span masonry arch road bridge of 18th/early 19th century date over the Triogue River.

History A bridge is shown on the 1839, 1888 and 1907 OS maps. It is explicitly captioned as Triogue Bridge on the 1907 edition.

Component LAIAR-008-024 1

Type Bridge (road/river) **Function** Road & pedestrian
Category Transport **Context** Infrastructure
Irish Grid 247574 207430 +/- 10m
Survey date 11/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge

Description A twin-arch bridge carries a public road over the Triogue River. Of random rubble masonry throughout. The arches are of segmental profile with dressed stone voussoirs. The left and right-bank arches span 3.16m and 3.14m respectively. There is a triangular masonry cutwater to the upstream (south) end of the pier. The downstream (north) spandrel and parapet have been partly replaced with mass concrete; the parapets are 6.76m apart. The abutments and soffits have been gunited. A pipe is carried across outside face of downstream parapet.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase LA Bridge R017; Mountmellick Canal F932

References

LAIAR-008-024_01 FWH 10/04/2007 Upstream (S) elevation from right bank.



LAIAR-008-024_02 FWH 14/06/2006 Upstream (S) elevation from left bank.



LAIAR-008-024_03 FWH 14/06/2006 Downstream (N) elevation.



Site no LAIAR-008-026

Supply Bridge

County	Laois	Townland	Kilnacash	Town
Planning	Laois CC	Discovery map	54	Six-inch map 8

Summary An arched masonry road bridge of c.1828 over a feeder to the Mountmellick Branch of the Grand Canal.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. This bridge is shown on the 1839 and 1888 OS maps and captioned Supply Bridge on the 1907 edition.

Component LAIAR-008-026 1

Type	Bridge (road/canal feeder)	Function	Inland waterway		
Category	Transport	Context	Infrastructure		
Irish Grid	247913 207431		+/- 10m		
Survey date	14/06/2006	Surveyor	Fred Hamond		
Remains	Some remains	Condition	Good	Current use	Road bridge
Description	A masonry arch bridge carries a public road over al feeder to the Mountmellick Canal. Now infilled and faces buried except for arch crown on north side. This appears to be of semicircular profile and 1.0-1.2m wide, with finely dressed limestone voussoirs. Random rubble parapet wall over. Otherwise marked by slight bump in road.				

Interest Historical

Evaluation Of local industrial heritage significance due to its historical association with the Mountmellick Canal.

Rating	Local	Protection		Action	None
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Associated with LAIAR-008-046

Other dbase Mountmellick Canal F308a

References

LAIAR-008-026_01 FWH 14/06/2006 North elevation of Supply Bridge.



Site no **LAIAR-008-027** **Kilnacash Bridge**

County Laois **Townland** Kilnacash **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 8

Summary An arched masonry road bridge of c.1828 over the Mountmellick Branch of the Grand Canal. Now by-passed.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. Kilnacash Bridge is captioned on the 1839, 1888 and 1907 OS maps.

Component **LAIAR-008-027 1**

Type Bridge (road/canal) **Function** Inland waterway
Category Transport **Context** Infrastructure
Irish Grid 248070 207602 +/- 10m
Survey date 14/06/2006 **Surveyor** Fred Hamond
Remains Complete **Condition** Fair **Current use** Disused

Description A masonry arch bridge carries a now-abandoned public road over the defunct Mountmellick Branch of Grand Canal. Of randomly coursed squared limestone rubble. The arch is of semicircular profile and measures 6.75m in span by 4.12m in depth. Its soffit is of squared rubble and the voussoirs and quoins are of finely dressed stone. A 2.01m wide towpath runs through the south side (the channel is 4.74m wide). Towrope marks visible on south-west quoin. Both faces of the bridge curve outwards to terminate in out-projecting piers embellished with finely dressed string course which continues around outside base of parapets. The inside faces of the parapets are of random rubble. Parapet on west side now partly missing. Surviving sections are coped with finely dressed limestone blocks. Humped deck. No datestones. Now by-passed to east by a road across the infilled bed. Heavy ivy overgrowth.

Interest Architectural; Historical

Evaluation Of architectural and historical merit. Regional industrial heritage significance. Merits inclusion in Record of Protected Structures.

Rating Regional **Protection** **Action** RPS

Associated with LAIAR-008-031

Other dbase IWAI MMC 180; Mountmellick Canal F307

References

LAIAR-008-027_01 FWH 14/06/2006 Kilnecash Bridge from west.



LAIAR-008-027_02 FWH 14/06/2006 Quoin detail at south-west corner of Kilnecash Bridge. Note towrope marks.



Site no LAIAR-008-028

County	Laois	Townland	Clonterry; Kilnacash; Kilmainham	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	8

Summary A masonry slab culvert of c.1828 carries a tributary of the Triogue River under the Mountmellick Branch of the Grand Canal.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. This aqueduct is shown on the 1839, 1888 and 1907 OS maps.

Component LAIAR-008-028 1

Type	Bridge (canal/river)	Function	Inland waterway		
Category	Transport	Context	Infrastructure		
Irish Grid	248378 207876		+/- 10m		
Survey date	14/06/2006	Surveyor	Fred Hamond		
Remains	Complete	Condition	Fair	Current use	Disused
Description	A 1.2m wide culvert carries a tributary of the Triogue River under the now-disused Mountmellick Branch of Grand Canal. North-west face inspected. Only its squared random rubble spandrel is now visible; probably flat stone heads between the abutments. Overgrown.				

Interest Historical

Evaluation Of local industrial heritage significance due to its historical association with the Mountmellick Canal.

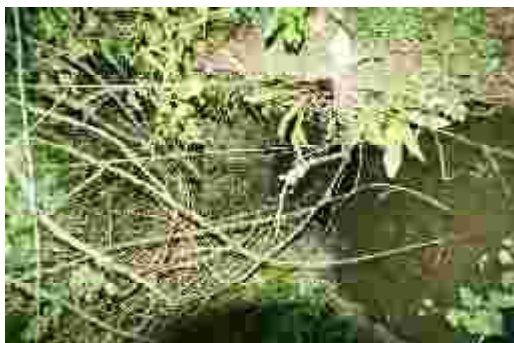
Rating	Local	Protection		Action	None
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Associated with LAIAR-008-031

Other dbase Mountmellick Canal F306

References

LAIAR-008-028_01 FWH 14/06/2006 North-west elevation of culvert.



Site no **LAIAR-008-029** **Dangan's Bridge**

County Laois **Townland** Dangan **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 8

Summary An arched masonry road bridge of c.1828 over the Mountmellick Branch of the Grand Canal.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. Dangan's Bridge is captioned on the 1839, 1888 and 1907 OS maps.

Component **LAIAR-008-029 1**

Type Bridge (road/canal) **Function** Inland waterway
Category Transport **Context** Infrastructure
Irish Grid 249013 208376 +/- 10m
Survey date 14/06/2006 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge

Description A masonry arch bridge carries a public road over a now-disused section of the Mountmellick Branch of the Grand Canal. Of randomly coursed squared limestone rubble. The arch is of semicircular profile measures 6.70m in span by 7.26m in depth. Its soffit is of squared rubble and the voussoirs and quoins are of finely dressed stone. A 1.74m wide towpath runs through the south side (the channel is 4.96m wide). Both faces of the bridge curve outwards to terminate in out-projecting piers embellished with finely dressed string course which continues around outside base of parapets. The inside faces of the parapets are of random rubble and are spaced at 6.50m; they are coped with finely dressed limestone blocks. Humped deck. No datestones. Heavy ivy overgrowth.

Interest Architectural; Historical

Evaluation Of architectural and historical merit. Regional industrial heritage significance. Warrants inclusion in Record of Protected Structures.

Rating Regional **Protection** **Action** RPS

Associated with LAIAR-008-031

Other dbase IWAI MMC 170; LA Bridge C018; Mountmellick Canal F305

References

LAIAR-008-029_01 FWH 14/06/2006 Dangan's Bridge: east elevation.



LAIAR-008-029_02 FWH 14/06/2006 Dangan's Bridge: inside of arch, from west.



LAIAR-008-029_03 FWH 14/06/2006 Towpath underneath Dangan's Bridge.



Site no **LAIAR-008-030** **Skeagh Bridge**

County Laois **Townland** Tinnakill (Portnahinch By) **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 8

Summary An arched masonry accommodation bridge of c.1828 over the Mountmellick Branch of the Grand Canal. Now by-passed.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. Skeagh Bridge is captioned on the 1839, 1888 and 1907 OS maps.

Component **LAIAR-008-030 1**

Type Bridge (road/canal) **Function** Inland waterway
Category Transport **Context** Infrastructure
Irish Grid 249488 208932 +/- 10m
Survey date 29/06/2006 **Surveyor** Fred Hamond
Remains Complete **Condition** Fair **Current use** Disused
Description A masonry arch bridge formerly carried an accommodation road over a now-disused section of the Mountmellick Branch of Grand Canal. Of randomly coursed squared limestone rubble. The arch is of semicircular profile but has been largely buried under later infill, along with most of the west face, from beneath the arch crown. The arch's soffit is of squared rubble and its voussoirs are of finely dressed stone. A now-buried towpath runs through its south side. Both faces of the bridge curve outwards to terminate in out-projecting piers embellished with finely dressed string course which continues around outside base of parapets. Some stone has been removed from the south-west pier. The parapets are coped with finely dressed limestone blocks and their inside faces are spaced at 3.32m. Humped deck. No datestones. Heavy ivy overgrowth on east face. The approach roads have been removed at both ends and the bridge has been superseded to its west by a metalled road across an infilled section of the canal.

Interest Architectural; Historical; Setting

Evaluation Despite being partly buried, sufficient is exposed to demonstrate this bridge's architectural merit. Its association with the Grand Canal Company is also of note. It is also a feature of the landscape hereabouts. Of regional industrial heritage significance. Merits inclusion in record of Protected Structures.

Rating Regional **Protection** **Action** RPS

Associated with LAIAR-008-031

Other dbase IWAI MMC 160; Mountmellick Canal F303

References

LAIAR-008-030_01 FWH 29/06/2006 Skeagh Bridge from south-east. Note damaged pier at right and removed approach road.



LAIAR-008-030_02 FWH 29/06/2006 West elevation of Skeagh Bridge.



LAIAR-008-030_03 FWH 29/06/2006 Skeagh Bridge from north-west.



LAIAR-008-030_04 FWH 29/06/2006 South-west terminal pier detail.



Site no LAIAR-008-039

County Laois **Townland** Coolnavarnoge and Coolaghy **Town**

Planning Laois CC **Discovery map** 55 **Six-inch map** 8

Summary Masonry arch road bridge over Dublin-Cork railway line, opened by Great Southern & Western Railway Co in 1847.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway was opened in 1847. An uncaptioned bridge is marked on the 1888 and 1907 OS maps.

Component LAIAR-008-039 1

Type Bridge (road/rail) **Function** Railway

Category Transport **Context** Infrastructure

Irish Grid 252232 208244 +/- 10m

Survey date 11/04/2007 **Surveyor** Fred Hamond

Remains Complete **Condition** Good **Current use** Road bridge

Description A single-span masonry arch bridge carries a road over the double-track Dublin-Cork railway. The abutments are of regularly-coursed rock-faced masonry blocks, with margined quoins. The arch is of semi-elliptical profile and measures 9.15m across. It is embellished with rusticated voussoirs (with margined edges to each block) which step into the spandrels. Its soffit is of stone blocks. A string course runs around the quoins and through the arch at spring level. The spandrels are of roughly-dressed and squared masonry laid to regular courses. A string course runs over the crown of the arch. The parapets are of dressed and squared blocks, randomly laid and with out-projecting coping stones; they are 6.70m apart. Wing walls project at right angles and are detailed as the parapets; they are coped with stone blocks. A galvanised metal staircase rises up the left wing wall as viewed from the Cork side, giving access to the track from the road. Signs on the abutment quoins note this bridge as Iarnród Éireann bridge no.126. The road approaches are ramped.

Interest Architectural; Historical

Evaluation This bridge is of architectural interest on account of the quality and detailing of the stonework. It is also of historical interest due to its association with the Great Southern & Western Railway Co. It is of local industrial heritage significance.

Rating Local **Protection** **Action** None

Associated with LAIAR-008-038

Other dbase

References

LAIAR-008-039_01 FWH 11/04/2007 SW elevation.



Site no LAIAR-008-040

County Laois **Townland** Coolnavarnoge and Coolaghy **Town**

Planning Laois CC **Discovery map** 54 **Six-inch map** 8

Summary Masonry arch road bridge over Dublin-Cork railway line, opened by Great Southern & Western Railway Co in 1847.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway was opened in 1847. An uncaptioned bridge is marked on the 1888 and 1907 OS maps.

Component LAIAR-008-040 1

Type Bridge (road/rail) **Function** Railway

Category Transport **Context** Infrastructure

Irish Grid 251946 207789 +/- 10m

Survey date 11/04/2007 **Surveyor** Fred Hamond

Remains Complete **Condition** Good **Current use** Road bridge

Description A single-span masonry arch bridge carries a road over the double-track Dublin-Cork railway. The abutments are of regularly-coursed rock-faced masonry blocks, with margined quoins. The arch is of semi-elliptical profile and measures 9.16m across. It is embellished with rusticated voussoirs (with margined edges to each block) which step into the spandrels. Its soffit is of stone blocks. A string course runs around the quoins and through the arch at spring level. The spandrels are of roughly-dressed and squared masonry laid to regular courses. A string course runs over the crown of the arch. The parapets are of dressed and squared blocks, randomly laid and with out-projecting coping stones; they are 3.87m apart. Sloping wing walls project at right angles. They are detailed as the parapets and are coped with stone blocks; they terminate in small square piers. Signs on the abutment quoins note this bridge as Iarnród Éireann bridge no.128. The road approaches are ramped.

Interest Architectural; Historical

Evaluation This bridge is of architectural interest on account of the quality and detailing of the stonework. It is also of historical interest due to its association with the Great Southern & Western Railway Co. It is of local industrial heritage significance.

Rating Local **Protection** **Action** None

Associated with LAIAR-008-038

Other dbase

References

LAIAR-008-040_01 FWH 11/04/2007 SW elevation.



Site no LAIAR-008-041

County	Laois	Townland	Ballykillane	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	8

Summary Masonry arch road bridge over Dublin-Cork railway line, opened by Great Southern & Western Railway Co in 1847. Soffit is of brick.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway was opened in 1847. An uncaptioned bridge is marked on the 1888 and 1907 OS maps.

Component LAIAR-008-041 1

Type	Bridge (road/rail)	Function	Railway		
Category	Transport	Context	Infrastructure		
Irish Grid	251678 207368 +/- 10m				
Survey date	11/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A single-span masonry arch bridge carries a road over the double-track Dublin-Cork railway. The abutments are of regularly-coursed rock-faced masonry blocks, with margined quoins. There are shallow buttresses to each side, detailed as the abutments. The arch is of semi-elliptical profile and measures 9.20m across. It is embellished with rusticated voussoirs (with margined edges to each block) which step into the spandrels. Its soffit is of brick. A string course runs around the quoins and through the arch at spring level. The spandrels are of roughly-dressed and squared masonry laid to regular courses. A string course runs over the crown of the arch. The parapets are of dressed and squared blocks, randomly laid and with out-projecting coping stones; they are 6.55m apart. They terminate in out-projecting piers which are upward continuations of the abutments. Signs on the abutment quoins note this bridge as Iarnród Éireann bridge no.129. The road approaches are ramped.				

Interest Architectural; Historical

Evaluation This bridge is of architectural interest on account of the quality and detailing of the stonework. It is also of historical interest due to its association with the Great Southern & Western Railway Co. It is of local industrial heritage significance.

Rating	Local	Protection		Action	None
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Associated with LAIAR-008-038

Other dbase

References

LAIAR-008-041_01 FWH 11/04/2007 NE elevation.



LAIAR-008-041_02 FWH 11/04/2007 Brick soffit detail.



Site no **LAIAR-008-043** **Carn Bridge**

County Laois **Townland** Carn or Curraghane **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 8

Summary Masonry arch road bridge over Dublin-Cork railway line, opened by Great Southern & Western Railway Co in 1847.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway was opened in 1847. An uncaptioned bridge is marked on the 1888 OS map and Cam Bridge on the 1907 edition.

Component **LAIAR-008-043 1**

Type Bridge (road/rail) **Function** Railway
Category Transport **Context** Infrastructure
Irish Grid 251166 206554 +/- 10m
Survey date 11/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge

Description A single-span masonry arch bridge carries a road over the double-track Dublin-Cork railway. The abutments are of regularly-coursed rock-faced masonry blocks, with margined quoins. There are shallow buttresses to each face, detailed as the abutments. The arch is of semi-elliptical profile and measures 9.12m across. It is embellished with rusticated voussoirs (with margined edges to each block) which step into the spandrels. Its soffit is of stone blocks. A string course runs around the quoins and through the arch at spring level. The spandrels are of roughly-dressed and squared masonry laid to regular courses. A string course runs over the crown of the arch. The parapets are of dressed and squared blocks, randomly laid and with out-projecting coping stones. They are 10.86m apart and terminate in out-projecting piers which are upward continuations of the buttresses. Curved and down-sloping wing walls project; they are detailed as the parapets and are coped with stone blocks. Signs on the abutment quoins note this bridge as Iarnród Éireann bridge no.131. The road approaches are ramped.

Interest Architectural; Historical

Evaluation This bridge is of architectural interest on account of the quality and detailing of the stonework. It is also of historical interest due to its association with the Great Southern & Western Railway Co. It is of local industrial heritage significance.

Rating Local **Protection** **Action** None

Associated with LAIAR-008-038

Other dbase

References

LAIAR-008-043_01 FWH 11/04/2007 SW elevation.



LAIAR-008-043_02 FWH 11/04/2007 Stonework detail at right-hand side of SW elevation.



Site no **LAIAR-008-045** **Derrydavy Bridge**

County Laois **Townland** Ballintaggart (Portnahinch By); Derrydavy **Town**

Planning Laois CC **Discovery map** 54 **Six-inch map** 8

Summary A masonry arch road bridge of 18th/early 19th century date over the Triogue River. The arch soffit has been rebuilt in concrete during the 20th century.

History A bridge is shown on the 1839, 1888 and 1907 OS maps. It is captioned as Derrydavy Bridge on the 1907 edition.

Component **LAIAR-008-045 1**

Type Bridge (road/river) Function Road & pedestrian

Category Transport Context Infrastructure

Irish Grid 247843 205719 +/- 10m

Survey date 11/04/2007 Surveyor Fred Hamond

Remains Complete Condition Good Current use Road bridge

Description A single-arch masonry bridge carries a public road over the Triogue River. The abutments, spandrels and parapets are of random rubble construction. The arch is of segmental profile and is of concrete, indicating a complete replacement of the original (presumably of random rubble); it spans 4.57m. The parapets are coped with concrete and are 5.03m apart. The deck has a slight curvature and the approaches are slightly dog-legged.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase

References

LAIAR-008-045_01 FWH 10/04/2007 Downstream (NW) elevation.



Site no LAIAR-008-049

Eyne Bridge

County	Laois	Townland	Eyne; Kyletalesha	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	8

Summary A single-arch 20th century masonry bridge carries a road over the Triogue River. To the west, a similar bridge of 18th or early 19th century date carries the road over the river's previous course before it was realigned.

History A bridge is shown on the 1839, 1888 and 1907 OS maps. It is captioned as Eyne Bridge on the 1907 edition. The river was subsequently straightened and a new bridge erected to span the new channel a short distance east of the original one (which is now over a dry channel).

Component LAIAR-008-049 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	247437 204180		+/- 10m		
Survey date	11/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A single-arch masonry bridge carries a public road over the now-defunct original channel of the Triogue River. It is of random rubble construction throughout. The arch is of segmental profile and is 3.81m wide. Concrete footings have been added to the abutments. This bridge now serves as a flood arch to the replacement bridge over the realigned river channel a short distance eastwards.				

Component LAIAR-008-049 2

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	247469 204178		+/- 10m		
Survey date	11/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A single-arch masonry bridge carries a public road over the realigned channel of the Triogue River. It is of random rubble construction throughout. The arch is of segmental profile and has split rubble voussoirs; it spans 5.12m. The parapets and part of the spandrels have been rebuilt in the relatively recent past. The parapets are 7.27m apart and are coped with concrete. The abutments have concrete footings and there are three metal tie bars between the spandrels.				

Interest Historical

Evaluation These two bridges are of some historical interest in reflecting the realignment of the Triogue River. However, they are of no special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase LA Bridge C025

References

LAIAR-008-049_01 FWH 10/04/2007 North elevation of original bridge (component 1).



LAIAR-008-049_02 FWH 10/04/2007 Upstream (S) elevation of later bridge (component 2).



Site no LAIAR-008-050 **Two Mile Bridge**

County Laois **Townland** Knocknagroagh; Kyletalesha **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 8

Summary A triple-span masonry arch road bridge of 18th/early 19th century date over the Triogue River.

History A bridge is shown on the 1839, 1888 and 1907 OS maps. It is captioned as Two Mile Bridge on the 1907 edition. So named because it is two Irish miles from Mountmellick (as is bridge LAIAR-003-010).

Component LAIAR-008-050 1

Type Bridge (road/river) **Function** Road & pedestrian
Category Transport **Context** Infrastructure
Irish Grid 247352 203593 +/- 10m
Survey date 11/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge

Description A triple-arch masonry bridge carries a public road over the Triogue River. It is of random rubble construction throughout. The arches are of semicircular profile. From the left to right banks, their spans are 3.19m, 3.13m and 1.72 respectively. The middle arch is slightly higher than its flankers, giving a curvature to the road deck. The original rubble stone soffit of this arch has been replaced with concrete. The right-bank arch is normally dry. The piers are relatively wide relative to the arch spans. Triangular concrete cutwaters have been added to their upstream (west) ends; these rise to just above arch spring level. Concrete has also been used along the abutment footings and on the bed of the river. The parapets are coped with concrete and are 4.81m apart.

Interest Architectural

Evaluation This is a good example of a triple-span rubble masonry bridge and is of local industrial heritage interest.

Rating Local **Protection** **Action** None

Associated with

Other dbase LA Bridge C026

References

LAIAR-008-050_01 FWH 10/04/2007 Upstream (W) elevation.



LAIAR-008-050_02 FWH 10/04/2007 Downstream (E) elevation.



Site no LAIAR-008-053

County	Laois	Townland	Straboe	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	8

Summary Masonry arch road bridge over Dublin-Cork railway line, opened by Great Southern & Western Railway Co in 1847.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway was opened in 1847. An uncaptioned bridge is marked on the 1888 and 1907 OS maps.

Component LAIAR-008-053 1

Type	Bridge (road/rail)	Function	Railway		
Category	Transport	Context	Infrastructure		
Irish Grid	249100 203225		+/- 10m		
Survey date	11/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge

Description A single-span masonry arch bridge carries a road over the double-track Dublin-Cork railway. The abutments are of regularly-coursed roughly-faced masonry blocks, with margined quoins. The arch is of semi-elliptical profile and has vee-joined rusticated voussoirs (with margined edges to each block). The soffit is of masonry blocks. A string course runs around the quoins and through the arch at spring level. The spandrels are of roughly-dressed squared random rubble set in regular courses. A string course runs over the crown of the arch. The parapets, which are 6.62m apart, are of dressed and squared rubble, randomly laid and coped with out-projecting dressed stone blocks. Wing walls project at right angles; these are of roughly-dressed squared random rubble brought to courses. Signs on the abutment quoins and parapets note this bridge as Iarnród Éireann bridge no.133. The approaches to the bridge are dog-legged and ramped. There is a memorial set into the roadside face of the NE parapet to commemorate the victim of an accident hereabouts.

Interest Architectural; Historical

Evaluation This bridge is of architectural interest on account of its high quality of construction, stonework detailing and embellishment. It also has a historical association with the Great Southern & Western Railway. It is of local industrial heritage significance.

Rating	Local	Protection		Action	None
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Associated with LAIAR-008-038

Other dbase

References

LAIAR-008-053_01 FWH 10/04/2007 From NE.



Site no LAIAR-008-072 **Nuns' Bridge**

County Laois **Townland** Ballycullenbeg; Townparks (Tinnahinch By) **Town** Mountmellick

Planning Laois CC **Discovery map** 54 **Six-inch map** 8

Summary Vestige of a concrete abutment to an early/mid 20th century single-span metal girder foot bridge over the Owenass River.

History This bridge is not shown on the 1839, 1888 or 1907 OS maps and is therefore of early/mid 20th century date. Locally known as the Nuns' Bridge as used by nuns from nearby convent. Demolished in 2006. Published photographs show it to have been a single-span metal girder foot bridge on concrete abutments and with three-bar metal railings to the parapets.

Component LAIAR-008-072 1

Type Bridge (foot/river) **Function** Road & pedestrian

Category Transport **Context** Infrastructure

Irish Grid 245437 207693 +/- 10m

Survey date 11/04/2007 **Surveyor** Fred Hamond

Remains Traces **Condition** Fair **Current use** Disused

Description Only the base of a mass-concrete abutment survives on the left bank.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase

References

Scott, M. Mountmellick Pictorial Memories: Book 2 (1998).

Page(s) 11

LAIAR-008-072_01 FWH 10/04/2007 Left bank abutment remains, from SE.



Site no **LAIAR-008-082**

County Laois **Townland** Coolnavarnoge and Coolaghy **Town**

Planning Laois CC **Discovery map** 54 **Six-inch map** 8

Summary Masonry arch accommodation bridge over Dublin-Cork railway line, opened by Great Southern & Western Railway Co in 1847.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway was opened in 1847. An uncaptioned bridge is marked on the 1888 and 1907 OS maps.

Component **LAIAR-008-082 1**

Type Bridge (road/rail) Function Railway

Category Transport Context Infrastructure

Irish Grid 252082 207999 +/- 10m

Survey date 11/04/2007 Surveyor Fred Hamond

Remains Complete Condition Good Current use Road bridge

Description A single-span masonry arch bridge carries an accommodation track over the double-line Dublin-Cork railway. The abutments are of regularly-coursed rock-faced masonry blocks, with margined quoins. There are shallow buttresses to each face, detailed as the abutments. The arch is of semi-elliptical profile and measures 9.14m across. It is embellished with rusticated voussoirs (with margined edges to each block) which step into the spandrels. Its soffit is of stone blocks. A string course runs around the quoins and through the arch at spring level. The spandrels are of roughly-dressed and squared masonry laid to regular courses. A string course runs over the crown of the arch. The parapets are of dressed and squared blocks, randomly laid and with out-projecting coping stones; they are 3.85m apart and terminate in out-projecting piers (upward continuations of the buttresses). Curved and down-sloping wing walls project at each end. They are detailed as the parapets, are coped with stone blocks and terminate in small square piers. Signs on the abutment quoins note this bridge as Iarnród Éireann bridge no.127.

Interest Architectural; Historical

Evaluation This bridge is of architectural interest on account of the quality and detailing of the stonework. It is also of historical interest due to its association with the Great Southern & Western Railway Co. It is of local industrial heritage significance.

Rating Local **Protection** **Action** None

Associated with

Other dbase

References

LAIAR-008-082_01 FWH 11/04/2007 SW elevation.



Site no LAIAR-008-083

County	Laois	Townland	Ballycullenbeg; Townparks (Tinnahinch By)	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	8

Summary An arched masonry bridge carries a public footpath over the Owenass River.

History A bridge is shown on the 1839 and 1888 OS maps. It is explicitly cited as a foot bridge on the 1907 OS map. According to Scott, the present masonry bridge is a replacement of an earlier timber one, but confirmation of this awaits future research. The bridge was refurbished in the relatively recent past to give public access to the playing fields on the right bank of the Owenass River.

Component LAIAR-008-083 1

Type	Bridge (foot/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	245257 207947	+/- 10m			
Survey date	11/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Excellent	Current use	Foot bridge
Description	An arched masonry bridge carries a public footpath over the Owenass River. The abutments are of squared random rubble laid to courses. The arch has a segmental profile and spans 8.96m; its voussoirs are of dressed stone blocks. The spandrels and parapets are of random rubble. The latter, which are 2.84m apart, have been rebuilt (along with sections of the spandrels) and are rendered with cement to their inside faces; their copings have been replaced with concrete. The abutments have concrete footings.				

Interest None

Evaluation No special industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with

Other dbase

References

Scott, M. Mountmellick Pictorial Memories: Book 2 (1998).

Page(s) 12

LAIAR-008-083_01 FWH 11/04/2007 Upstream (S) elevation.



Site no LAIAR-008-084

County	Laois	Townland	Kilmainham	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	8

Summary Site of culvert of c.1828 carrying a feeder to the Mountmellick Canal under a minor road.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. This bridge over one of the canal's feeders is shown on the 1839, 1888 and 1907 OS maps.

Component LAIAR-008-084 1

Type	Bridge (road/canal feeder)	Function	Inland waterway		
Category	Transport	Context	Infrastructure		
Irish Grid	247822 206716		+/- 10m		
Survey date	30/08/2006	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	Line of feeder infilled to both sides. No visible traces of culvert which formerly carried the feeder under a minor road. May possibly be buried.				

Interest None

Evaluation No industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with

Other dbase

References

Site no LAIAR-008-085

County	Laois	Townland	Brittas (Portnahinch By)	Town
Planning	Laois CC	Discovery map	54	Six-inch map 8

Summary Site of culvert of c.1828 carrying a feeder to the Mountmellick Canal under a minor road.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. This bridge over one of the canal's feeders is shown on the 1839, 1888 and 1907 OS maps.

Component LAIAR-008-085 1

Type	Bridge (road/canal feeder)	Function	Inland waterway		
Category	Transport	Context	Infrastructure		
Irish Grid	248005 205905		+/- 10m		
Survey date	30/08/2006	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	The original culvert which carried a feeder to the Mountmellick Canal under a minor road has been replaced in the relatively recent past by a concrete pipe.				

Interest None

Evaluation No industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with

Other dbase

References

Site no **LAIAR-008-086**

County	Laois	Townland	Killeen or Killeenlynagh (Portnahinch By)	Town
Planning	Laois CC	Discovery map	54	Six-inch map 8

Summary Single-span bridge on Dublin-Cork railway line over accommodation track. Line opened by Great Southern & Western Railway Co in 1847.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway was opened in 1847. An uncaptioned bridge is marked on the 1888 and 1907 OS maps.

Component **LAIAR-008-086 1**

Type	Bridge (rail/road)	Function	Railway
Category	Transport	Context	Infrastructure
Irish Grid	250240 205084	+/- 10m	
Survey date		Surveyor	
Remains		Condition	Current use
Description	Not surveyed.		

Interest

Evaluation Not assessed.

Rating	Protection	Action
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Associated with

Other dbase

References

Site no LAIAR-008-087

County	Laois	Townland	Ballykillane	Town
Planning	Laois CC	Discovery map	54	Six-inch map 8

Summary Single-span bridge on Dublin-Cork railway line over accommodation track. Line opened by Great Southern & Western Railway Co in 1847.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway was opened in 1847. An uncaptioned bridge is marked on the 1888 and 1907 OS maps.

Component LAIAR-008-087 1

Type	Bridge (rail/road)	Function	Railway
Category	Transport	Context	Infrastructure
Irish Grid	251404 206933	+/- 10m	
Survey date		Surveyor	
Remains		Condition	Current use
Description	Not surveyed.		

Interest

Evaluation Not assessed.

Rating	Protection	Action
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Associated with

Other dbase

References

Site no **LAIAR-008-088**

County	Laois	Townland	Dangans; Tinnakill (Portnahinch By)	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	8

Summary Piped metal culvert carrying a tributary of the Triogue River under the Mountmellick Canal.

History The Mountmellick Canal was constructed in 1827-29 by the Grand Canal Company. It opened in 1831 and operated until 1960. This aqueduct is shown on the 1839, 1888 and 1907 OS maps. Metal pipe subsequently inserted.

Component **LAIAR-008-088 1**

Type	Bridge (canal/river)	Function	Inland waterway		
Category	Transport	Context	Infrastructure		
Irish Grid	249119 208451		+/- 10m		
Survey date	29/06/2006	Surveyor	Fred Hamond		
Remains	Complete	Condition	Fair	Current use	Culvert
Description	A pipe carries a tributary of the Triogue River under the Mountmellick Canal. North-west elevation inspected. Comprises a c.60cm diameter metal pipe. Mass concrete spandrel without parapet. Channel now very silted up.				

Interest Historical

Evaluation The north-west end of this culvert is undoubtedly a replacement of the original culvert (parts of which probably still survive under the canal). Of local industrial heritage significance due to association with Mountmellick Canal.

Rating	Local	Protection		Action	None
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Associated with

Other dbase

References

LAIAR-008-088_01 FWH 29/06/2006 North-west elevation of culvert.



Site no LAIAR-009-003

County Laois **Townland** Bolnagree; Ballintogher **Town**
Planning Laois CC **Discovery map** 55 **Six-inch map** 9

Summary An arched masonry culvert, built c.1790, carries a tributary of the River Barrow under the Athy Branch of the Grand Canal.

History This stretch of the Athy Branch of the Grand Canal, between Monasterevin and Athy, was constructed by the Grand Canal Company in 1785-91. This aqueduct is captioned on the 1839, 1888 and 1907 OS maps.

Component LAIAR-009-003 1

Type Bridge (canal/river) **Function** Inland waterway
Category Transport **Context** Infrastructure
Irish Grid 260811 208562 +/- 10m
Survey date 23/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Culvert
Description An arched masonry culvert carries a tributary of the River Barrow under the Athy Branch of the Grand Canal. The abutments are of squared rubble masonry, laid randomly. The arch is of segmental profile and spans 1.83m. The spandrels are also of squared random rubble. The wing walls are similarly detailed, but laid to courses.

Interest Historical

Evaluation Of historical interest due to its association with the Athy Branch of the Grand Canal, this culvert is of local industrial heritage significance.

Rating Local **Protection** **Action** None

Associated with LAIAR-009-002

Other dbase

References

LAIAR-009-003_01 FWH 23/04/2007 NW elevation.



Site no	LAIAR-009-005	Cappakeel Bridge
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County	Laois	Townland	Cappakeel; Rossmore (Stradbally By)	Town	
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Planning	Laois CC	Discovery map	55	Six-inch map	9
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Summary A concrete pipe culvert carries the Glasha River under a public road. It replaces a previous one of 18th/early 19th century date.

History A bridge is shown on the 1839, 1888 and 1907 OS maps. It is captioned as Cappakeel Bridge on the 1907 edition. The present bridge is a later 20th century replacement of the previous one.

Component LAIAR-009-005 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	257468 203459		+/- 10m		
Survey date	23/04/2007	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	This bridge was replaced in the later 1900s with the present concrete one (component 2).				

Component LAIAR-009-005 2

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	257468 203458		+/- 10m		
Survey date	23/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A concrete pipe carries the Glasha River under a public road. The pipe is 1.46m in diameter. There are no parapets to this bridge.				

Interest None

Evaluation No special industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with

Other dbase

References

LAIAR-009-005_01 FWH 23/04/2007 Downstream (NE) elevation.



Site no	LAIAR-009-006	Rossmore Bridge
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County	Laois	Townland	Bellegrove; Rossmore (Stradbally By)	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	9

Summary A reinforced-concrete slab bridge over the Glasha River. It replaces a previous bridge of 18th/early 19th century date. Possibly built c.1930 by Board of Works in connection with Barrow Drainage Scheme.

History A bridge is shown on the 1839, 1888 and 1907 OS maps. It is captioned as Rossmore Bridge on the 1907 edition. The present bridge is a later 20th century replacement of the previous one. May be associated with Barrow Drainage Scheme of 1926-34.

Component LAIAR-009-006 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	258185 203904		+/- 10m		
Survey date	23/04/2007	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	This bridge was replaced in the later 1900s with the present concrete one (component 2).				

Component LAIAR-009-006 2

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	258185 203903		+/- 10m		
Survey date	23/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A concrete slab bridge carries a public road over the Glasha River. The abutments are of concrete and 9.00m deep. The reinforced-concrete slab deck spans 2.98m. It has no parapets. The right-bank abutment has subsided, causing a gap to open up between it and the deck.				

Interest	None				
Evaluation	No special industrial heritage significance.				
Rating	Record only	Protection		Action	None

Associated with

Other dbase

References

LAIAR-009-006_01 FWH 23/04/2007 Upstream (W) elevation.



Site no LAIAR-009-007

Sallyford Bridge

County	Laois	Townland	Courtwood; Fisherstown; Kilbrackan; Rathronshin	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	9

Summary An arched masonry culvert of late 18th/early 19th century date, carries the Sallyford Stream under a road.

History Sallyford Bridge is captioned on the 1839, 1888 and 1907 OS maps.

Component LAIAR-009-007 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	261075 205169 +/- 10m				
Survey date	23/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A masonry arch culvert carries the Sallyford Stream under a public road. It is of random rubble construction throughout. The arch is of segmental profile and spans 1.40m. The parapets, which are coped with concrete, are 6.82m apart.				

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase

References

LAIAR-009-007_01 FWH 23/04/2007 Upstream (W) elevation.



Site no **LAIAR-009-010** **Fisherstown Bridge**

County Laois **Townland** Fisherstown **Town**
Planning Laois CC **Discovery map** 55 **Six-inch map** 9

Summary An arched masonry road bridge, built c.1790, over the Athy Branch of the Grand Canal.

History This stretch of the Athy Branch of the Grand Canal, between Monasterevin and Athy, was constructed by the Grand Canal Company in 1785-91. This bridge is shown on the 1839 and 1888 OS maps and cited as Fisherstown Bridge on the 1907 edition.

Component **LAIAR-009-010 1**

Type Bridge (road/canal) **Function** Inland waterway
Category Transport **Context** Infrastructure
Irish Grid 262306 205706 +/- 10m
Survey date 23/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge

Description A masonry arch bridge carries a public road over the Athy Branch of Grand Canal. The abutments are of roughly-dressed masonry blocks, laid to courses. The arch is of segmental profile and spans 6.69m. Its voussoirs and soffit are of dressed stone blocks. The canal is 4.73m wide and the towpath through its west side is 1.96m. The spandrels are detailed as the abutments. The parapets are of random rubble and are 6.36m apart. There are outward curving random rubble wing walls at both ends. Those to the north side of the bridge are coped with masonry blocks, and those to the south with concrete. They terminate in small piers detailed as the abutments. The approaches are ramped and there is a curve to the deck.

Interest Architectural; Historical

Evaluation This bridge is of architectural merit as a typical canal bridge, and also of historical interest due to its canal association. It is of regional industrial heritage significance and merits its current inclusion in the Record of Protected Structures (LA 562).

Rating Regional **Protection** RPS LA 562 **Action** None (in RPS)

Associated with LAIAR-009-002

Other dbase LA Bridge C110; NIAH 12800902 (R)

References

Survey of the Architectural Heritage of County Laois (Dublin: NIAH, 2002). Page(s)

Waterways Ireland. Grand Canal Barrow Line: Fisherstown Bridge - Inventory and Condition Survey (Collins Engineers, 2007). Page(s)

LAIAR-009-010_01 FWH 23/04/2007 North elevation.



LAIAR-009-010_02 FWH 23/04/2007 South elevation.



Site no LAIAR-009-011

County Laois **Townland** Courtwood; Fisherstown **Town**
Planning Laois CC **Discovery map** 55 **Six-inch map** 9

Summary An arched masonry culvert, built c.1790, carries the Sallyford Stream under the Athy Branch of the Grand Canal.

History This stretch of the Athy Branch of the Grand Canal, between Monasterevin and Athy, was constructed by the Grand Canal Company in 1785-91. This aqueduct is captioned on the 1839, 1888 and 1907 OS maps.

Component LAIAR-009-011 1

Type Bridge (canal/river) **Function** Inland waterway
Category Transport **Context** Infrastructure
Irish Grid 262065 204844 +/- 10m
Survey date 23/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Culvert
Description An arched masonry culvert carries the Sallyford Stream under the Athy Branch of the Grand Canal. Only the arch and spandrels were visible. The former is of segmental profile and spans 1.52m. The spandrel is of roughly-dressed masonry blocks, laid to courses.

Interest Historical

Evaluation Of historical interest due to its association with the Athy Branch of the Grand Canal, this culvert is of local industrial heritage significance.

Rating Local **Protection** **Action** None

Associated with LAIAR-009-002

Other dbase

References

LAIAR-009-011_01 FWH 23/04/2007 Upstream (W) elevation.



Site no LAIAR-009-012 **Courtwood Bridge**

County Laois **Townland** Courtwood **Town**
Planning Laois CC **Discovery map** 55 **Six-inch map** 9

Summary An arched masonry road bridge, built c.1790, over the Athy Branch of the Grand Canal.

History This stretch of the Athy Branch of the Grand Canal, between Monasterevin and Athy, was constructed by the Grand Canal Company in 1785-91. Courtwood Bridge is captioned on the 1839, 1888 and 1907 OS maps.

Component LAIAR-009-012 1

Type Bridge (road/canal) **Function** Inland waterway
Category Transport **Context** Infrastructure
Irish Grid 261898 204120 +/- 10m
Survey date 23/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge
Description A masonry arch bridge carries a public road over the Athy Branch of Grand Canal. The abutments are of dressed masonry blocks, laid to courses. There is a slot up the middle of the east abutment. Its function is uncertain and there is no corresponding slot in the opposite side of the canal bank. The arch is of segmental profile and spans 6.50m. Its voussoirs and soffit are of dressed stone blocks. The canal is 4.44m wide and the towpath through its west side is 2.06m. The north spandrel and parapet are of squared random rubble, whereas they are of random rubble on the south face. The parapets are 6.40m apart. There are outward curving random rubble wing walls at both ends, terminating in square piers. The approaches are ramped and slightly dog-legged. The carriageway has a pronounced curvature. Just beyond the end of the bridge on its north side are opposite stop-plank slots.

Interest Architectural; Historical

Evaluation This bridge is of architectural merit as a typical canal bridge, and also of historical interest due to its canal association. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.

Rating Regional **Protection** **Action** RPS

Associated with LAIAR-009-002

Other dbase LA Bridge C111

References

Waterways Ireland. Grand Canal Barrow Line: Courtwood Bridge - Inventory Page(s) and Conditon Survey (Collins Engineers, 2007).

LAIAR-009-012_01 FWH 23/04/2007 South elevation.



LAIAR-009-012_02 FWH 23/04/2007 Stop sluice slots in east side of abutment.



Site no LAIAR-009-015

Ballyshaneduff Bridge

County Laois **Townland** Ballybrittas; Ballyshaneduff or The Derries **Town**

Planning Laois CC **Discovery map** 55 **Six-inch map** 9

Summary A culvert carries a tributary of the Glasha River under a public road. Of the 18th/early 19th century structure, only the abutments survive, the span having been replaced with a concrete arch. It was also widened with a reinforced-concrete slab deck in the later 1900s.

History A bridge is shown on the 1839, 1888 and 1907 OS maps. It is captioned as Ballyshaneduff Bridge on the 1907 edition. It was widened in the later 1900s as part of the road's upgrading.

Component LAIAR-009-015 1

Type Bridge (road/river) **Function** Road & pedestrian

Category Transport **Context** Infrastructure

Irish Grid 256713 206075 +/- 10m

Survey date 23/04/2007 **Surveyor** Fred Hamond

Remains Complete **Condition** Good **Current use** Road bridge

Description A arched concrete culvert carries a tributary of the Glasha River under a public road. The abutments are of random rubble and 11.55m deep. The previous span has been replaced with a 1.35m wide concrete arch of semicircular profile. The spandrels are also of rubble, as is the surviving parapet on the downstream (SE) end. The upstream end has been widened by 10.54m with a 1.42m wide reinforced-concrete slab deck over concrete abutments. The latter retain falsework sockets immediately under the deck. There is no parapet to the widened end.

Interest None

Evaluation No special industrial heritage significance.

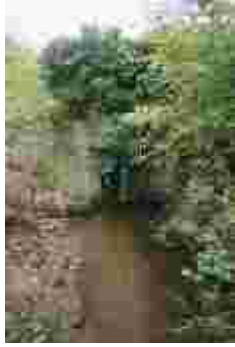
Rating Record only **Protection** **Action** None

Associated with

Other dbase

References

LAIAR-009-015_01 FWH 23/04/2007 Upstream (NW) elevation.



LAIAR-009-015_02 FWH 23/04/2007 Looking downstream inside upstream extension. Note falsework sockets.



Site no LAIAR-009-017

County Laois; Kildare **Townland** Killinure (Portnahinch By); Co Kildare **Town**

Planning Laois CC; Kildare CC **Discovery map** 55 **Six-inch map** 9

Summary A circular masonry pipe culvert, built c.1790, carries a tributary of the River Barrow under the Athy Branch of the Grand Canal on boundary with Co Kildare.

History This stretch of the Athy Branch of the Grand Canal, between Monasterevin and Athy, was constructed by the Grand Canal Company in 1785-91. This aqueduct is captioned on the 1839, 1888 and 1907 OS maps.

Component LAIAR-009-017 1

Type Bridge (canal/river) Function Inland waterway

Category Transport Context Infrastructure

Irish Grid 261299 209062 +/- 10m

Survey date 23/04/2007 Surveyor Fred Hamond

Remains Complete Condition Good Current use Culvert

Description A circular masonry pipe carries a tributary of the River Barrow under the Athy Branch of the Grand Canal. The culvert is 1.22m in diameter and comprises masonry blocks, roughly dressed to the curve on their inside faces.

Interest Architectural; Historical

Evaluation This is a good example of a masonry pipe culvert and of historical interest due to its association with the Athy Branch of the Grand Canal. It is of local industrial heritage significance.

Rating Local **Protection** **Action** None

Associated with LAIAR-009-002

Other dbase

References

LAIAR-009-017_01 FWH 23/04/2007 NW elevation.



LAIAR-009-017_02 FWH 23/04/2007 Looking through pipe from NW end.



Site no LAIAR-009-018

County Laois **Townland** Jamestown or Ballyteigeduff **Town**
Planning Laois CC **Discovery map** 55 **Six-inch map** 9

Summary An arched masonry culvert, built c.1790, carries a tributary of the River Barrow under the Athy Branch of the Grand Canal.

History This stretch of the Athy Branch of the Grand Canal, between Monasterevin and Athy, was constructed by the Grand Canal Company in 1785-91. This aqueduct is captioned on the 1839, 1888 and 1907 OS maps.

Component LAIAR-009-018 1

Type Bridge (canal/river) **Function** Inland waterway
Category Transport **Context** Infrastructure
Irish Grid 261583 206852 +/- 10m
Survey date 23/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Culvert

Description An arched masonry culvert carries a tributary of the River Barrow under the Athy Branch of the Grand Canal. Only the arch and spandrels were visible. The former is of segmental profile and spans 1.52m. The spandrel is of dressed masonry blocks, laid to courses. The angled wing walls are similarly detailed.

Interest Historical

Evaluation Of historical interest due to its association with the Athy Branch of the Grand Canal, this culvert is of local industrial heritage significance.

Rating Local **Protection** **Action** None

Associated with LAIAR-009-002

Other dbase

References

LAIAR-009-018_01 FWH 23/04/2007 SW elevation.



Site no LAIAR-009-019

County	Laois	Townland	Courtwood	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	9

Summary A circular masonry pipe culvert, built c.1790, carries a tributary of the River Barrow under the Athy Branch of the Grand Canal.

History This stretch of the Athy Branch of the Grand Canal, between Monasterevin and Athy, was constructed by the Grand Canal Company in 1785-91. This aqueduct is captioned on the 1839, 1888 and 1907 OS maps.

Component LAIAR-009-019 1

Type	Bridge (canal/river)	Function	Inland waterway		
Category	Transport	Context	Infrastructure		
Irish Grid	261820 203769		+/- 10m		
Survey date	23/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Culvert
Description	A circular masonry pipe carries a tributary of the River Barrow under the Athy Branch of the Grand Canal. Access to its east end was denied by impenetrable undergrowth, but the culvert is estimated to be c.0.60m in diameter.				

Interest Historical

Evaluation Of historical interest due to its association with the Athy Branch of the Grand Canal, this culvert is of local industrial heritage significance.

Rating	Local	Protection		Action	None
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Associated with LAIAR-009-002

Other dbase

References

Site no LAIAR-009-024

Wooden Bridge

County	Laois	Townland	Killaglish	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	9

Summary A single-span metal girder accommodation bridge over the Athy Branch of Grand Canal. The abutments probably date to the canal's construction c.1790, but the deck is a 20th century replacement.

History This stretch of the Athy Branch of the Grand Canal, between Monasterevin and Athy, was constructed by the Grand Canal Company in 1785-91. A wooden bridge is captioned on the 1839 and 1888 OS maps. An uncaptioned bridge is depicted on the 1907 edition. The present deck is a 20th century replacement.

Component LAIAR-009-024 1

Type	Bridge (road/canal)	Function	Inland waterway		
Category	Transport	Context	Infrastructure		
Irish Grid	260777 208054		+/- 10m		
Survey date	23/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A single-span metal beam bridge carries an accommodation track over the Athy Branch of the Grand Canal. The abutments, which are slightly battered, are of random rubble construction. The deck comprises five RSJs with timber planks over and is evidently a replacement. It spans 11.05m and is 3.33m wide. The parapets comprise three-bar steel railings. Curved random rubble wing walls. The east ends of the parapets have been rebuilt.				

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with LAIAR-009-002

Other dbase

References

Waterways Ireland. Grand Canal Barrow Line: Old Bridge/ Wooden Bridge - Page(s)
Inventory and Condition Survey (Collins Engineers, 2007).

LAIAR-009-024_01 FWH 23/04/2007 South elevation.



Site no LAIAR-009-027

County	Laois	Townland	Emo Park	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	9

Summary Site of foot bridge over stream in Emo Demesne.

History Cited as a wooden bridge on the 1839 OS map. A bridge is shown but not captioned on the 1888 and 1907 OS maps.

Component LAIAR-009-027 1

Type	Bridge (foot/river)	Function	Road & pedestrian		
Category	Transport	Context	Settlement		
Irish Grid	254487 206574		+/- 10m		
Survey date	23/04/2007	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	No traces of this bridge were evident.				

Interest None

Evaluation No industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with

Other dbase

References

Site no LAIAR-009-028

County	Laois	Townland	Emo Park	Town
Planning	Laois CC	Discovery map	55	Six-inch map 9

Summary Site of foot bridge over stream in Emo Demesne.

History Cited as a wooden bridge on the 1839 OS map. A bridge is shown but not captioned on the 1888 and 1907 OS maps.

Component LAIAR-009-028 1

Type	Bridge (foot/river)	Function	Road & pedestrian		
Category	Transport	Context	Settlement		
Irish Grid	254119 207509		+/- 10m		
Survey date	23/04/2007	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	No traces of this bridge were evident.				

Interest None

Evaluation No industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with

Other dbase

References

Site no **LAIAR-011-002** **Monicknew Bridge**

County Laois **Townland** Brockagh; Monicknew **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 11

Summary A high masonry arch road bridge of c.1840 over a tributary of the Delour River.

History A bridge is first shown here on the 1888 OS map. As it is not on the 1838 edition, it is therefore of mid 19th century date (according to Scully it was erected in 1840). It is captioned as Monicknew Bridge on the 1907 map.

Component **LAIAR-011-002 1**

Type Bridge (road/river) **Function** Road & pedestrian
Category Transport **Context** Infrastructure
Irish Grid 230796 202290 +/- 10m
Survey date 24/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge

Description An arched masonry bridge carries a public road over a tributary of the Delour River, here running in a deep valley. It is random rubble construction throughout. The stonework of the abutments has been brought to courses. There are three rows of falsework sockets on each abutment. They have dressed stone quoins and their outside faces taper inwards as they rise. There are similarly detailed buttresses to either side. The arch is of semicircular profile and spans 4.29m. For its width, it is relatively high, being 8.35m from the bed of the river to intrados of the arch. A decorative string course runs across the face of the abutments and around the quoins at arch spring level (but not through the arch). There is a similar string course over the crown of the arch. The parapets are 6.28m apart. The upstream one has been rebuilt in concrete and its road elevation faced with random rubble. They are both coped with split stones laid on edge. The bed of the river has been pitched with stones. A recreational footpath runs through the arch on the right bank.

Interest Architectural; Setting

Evaluation This bridge is a good example of a mid 19th century bridge in an impressive setting, adjacent to a recreational amenity area. It is of regional industrial heritage significance and merits its current inclusion in the Record of Protected Structures (LA 577).

Rating Regional **Protection** RPS LA 577 **Action** None (in RPS)

Associated with

Other dbase LA Bridge C080; NIAH 12801101 (R)

References

Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002). Page(s) 83
Survey of the Architectural Heritage of County Laois (Dublin: NIAH, 2002). Page(s)

LAIAR-011-002_01 FWH 24/04/2007 Upstream (N) elevation.



LAIAR-011-002_02 FWH 24/04/2007 Downstream (S) elevation.



LAIAR-011-002_03 FWH 24/04/2007 Falsework sockets on left-bank abutment.



LAIAR-011-002_04 FWH 24/04/2007 Stone pitching on bed of river.



Site no LAIAR-011-003 **Dooley's Bridge; {Killinure Bridge}**

County Laois **Townland** Inchanisky **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 11

Summary A wide masonry arch road bridge of mid 18th century date over the Delour River.

History A bridge is shown on the 1838, 1888 and 1907 OS maps. It is cited as Killinure Bridge on the 1838 map, and as Dooley's Bridge on the 1907 edition. According to Scully, it was built in the early 1750s.

Component LAIAR-011-003 1

Type Bridge (road/river) **Function** Road & pedestrian
Category Transport **Context** Infrastructure
Irish Grid 230179 200416 +/- 10m
Survey date 24/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge

Description An arched masonry span carries a public road over the Delour River. The abutments are of dressed and squared masonry, randomly laid. There are tapered buttresses to either side, with roughly dressed quoins. The arch is of segmental profile and spans 9.20m. A thin stringcourse runs over its crown. The spandrels and parapets are of random rubble; the latter are spaced at 6.70m. The road has a slight curvature. The bed of the river is pitched with stones, as are the abutment footings.

Interest Architectural; Historical

Evaluation This is a substantial example of a mid 18th century road bridge, notable for its wide span (over 9m/ 30ft). It is of regional industrial heritage interest and merits inclusion in the Record of Protected Structures.

Rating Regional **Protection** **Action** RPS

Associated with

Other dbase LA Bridge R078

References

Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002). Page(s) 145

LAIAR-011-003_01 FWH 24/04/2007 Upstream (NE) elevation.



LAIAR-011-003_02 FWH 24/04/2007 Looking NW across deck. Note curvature and edge-laid parapet copings.



LAIAR-011-003_03 FWH 24/04/2007 Stone pitching to foot of right-bank abutment.



Site no **LAIAR-011-005** **Derrycarrow Bridge**

County Laois **Townland** Aghduff; Derrycarrow **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 11

Summary An arched masonry bridge of 18th/early 19th century date over a tributary of the Delour River.

History A bridge is shown on the 1838 and 1888 maps. It is cited as Derrycarrow Bridge on the 1907 edition.

Component **LAIAR-011-005 1**

Type Bridge (road/river) Function Road & pedestrian
Category Transport Context Infrastructure
Irish Grid 225448 196256 +/- 10m
Survey date 14/06/2007 Surveyor Fred Hamond
Remains Complete Condition Good Current use Road bridge
Description An arched masonry bridge carries a road over a tributary of the Delour River. The abutments are of dressed masonry blocks laid to courses. The arch is of segmental profile and spans 5.62m. The spandrels are of squared random rubble brought to courses. The parapets are of random rubble and 6.01m apart. The angled wing walls are detailed as the spandrels. The bed of the river is paved.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase LA Bridge C095

References

Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002). Page(s) 122

LAIAR-011-005_01 FWH 14/06/2007 Upstream (N) elevation.



Site no LAIAR-011-007 **Cappanarrow Bridge**

County Laois **Townland** Cappanarrow; Derrycarrow **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 11

Summary An arched masonry road bridge of 18th/early 19th century date over a tributary of the Delour River.

History A bridge is shown on the 1838 and 1888 maps. It is cited as Cappanarrow Bridge on the 1907 edition.

Component LAIAR-011-007 1

Type Bridge (road/river) **Function** Road & pedestrian
Category Transport **Context** Infrastructure
Irish Grid 226237 196822 +/- 10m
Survey date 14/06/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge
Description An arched masonry bridge carries a road over a tributary of the Delour River. It is of random rubble masonry throughout. The arch is of semicircular profile and spans 4.46m. The parapets, which are coped with large roughly-dressed blocks, are 4.98m apart.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase LA Bridge C094

References

Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002). Page(s) 93

LAIAR-011-007_01 FWH 14/06/2007 Upstream (W) elevation.



Site no **LAIAR-011-008** **Cardtown Bridge**

County Laois **Townland** Cummer; Johnsborough;
Killeen (Upperwoods By) **Town**

Planning Laois CC **Discovery map** 54 **Six-inch map** 11

Summary A triple-arched masonry road bridge of 18th/early 19th century date over the Killeen River.

History A bridge is shown on the 1838, 1888 and 1907 OS maps. It is cited as Cardtown Bridge on the 1907 edition.

Component **LAIAR-011-008 1**

Type Bridge (road/river) Function Road & pedestrian

Category Transport Context Infrastructure

Irish Grid 227163 197959 +/- 10m

Survey date 24/04/2007 Surveyor Fred Hamond

Remains Complete Condition Good Current use Road bridge

Description A triple-arched masonry road bridge over the Killeen River. It is of split random rubble construction throughout. The abutments have dressed stone quoins. The left-bank abutment has a buttress on its downstream (south) face. There are triangular cutwaters to the upstream ends of the piers. These rise to arch spring level and are of squared and coursed masonry blocks. The arches are of semicircular profile. From the left to right banks, their spans are 3.01m, 2.97m and 3.05m. The left-bank arch is dry in normal circumstances. The parapets are 5.73m apart and are coped with edge-laid stones. The bed is pitched with stones.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase LA Bridge C093

References

Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002). Page(s) 95

LAIAR-011-008_01 FWH 24/04/2007 Downstream (SE) elevation.



Site no LAIAR-011-009 **Lacca Bridge; {Bennett's Bridge}**

County	Laois	Townland	Lacka	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	11

Summary A metal girder road bridge over the Delour River, erected by Messrs Raybould (Workington) ic.1900. It replaced an 18th/early 19th century multi-span bridge.

History Bennett's Bridge is cited on the 1838 and 1888 OS maps. It is depicted as a multi-span structure. It is captioned as Lacca Bridge on the 1907 map. The latter is the present metal girder bridge which was manufactured by B.J. Raybould & Co Ltd of Workington in the late 1800s or early 1900s.

Component LAIAR-011-009 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	229171 198055		+/- 10m		
Survey date	24/04/2007	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	There are no obvious traces of the 18th/early 19th century bridge which preceded the present one (except possibly for the approaches).				

Component LAIAR-011-009 2

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	229170 198055		+/- 10m		
Survey date	24/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Fair	Current use	Road bridge
Description	A single metal girder span carries a public road over the Delour River. The abutments are of dressed and squared rubble, the outside faces of which taper inwards as they rise. The span consists of two metal plate girders set 3.66m apart (90 x 30 cm end cross section). Each beam is fabricated from three smaller lengths riveted together and their webs have been strengthened with vertical angle pieces affixed to their outside faces. The ends of the beams rest on concrete pillow blocks at the tops of the abutments. Set between these beams are 20cm x 10cm I beams at 1.10m centres. Between these smaller beams are concrete jack arches. The deck spans 15.25m and is 4.00m wide. The parapets comprise two-bar tubular steel railings supported on cast-iron T-section posts. Intermediate angled-steel posts have subsequently been added as strengtheners and they terminate in random rubble piers. A plaque on the river face of the downstream (SW) girder reads 'B.J. Raybould & Co Ld/ Makers/ Workington'. There was probably a plaque on the upstream face as well, but it has been removed. Part of the upstream railing and the upstream left-bank terminal pier have been damaged by vehicle impact. On the eastern approach to the bridge is a rubble masonry flood arch of semicircular profile and 2.21m span.				

Interest Architectural; Historical; Rarity; Setting; Technical

Evaluation This bridge is of architectural and technical interest on account of its construction (longitudinal and transverse metal beams and concrete jack arches), the only such example in the county. It also has an attested maker and can be attributed to the decades either side of 1900. It has a picturesque setting beside a picnic amenity area on the upstream right bank. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.

Rating Regional **Protection** **Action** RPS

Associated with

Other dbase LA Bridge C278

References

Drawings (9 no) of Lacca Bridge for Laois CC.	Page(s)
Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002).	Page(s) 162

LAIAR-011-009_01 FWH 24/04/2007 Upstream (NE) elevation.



LAIAR-011-009_02 FWH 24/04/2007 Downstream (SW) elevation.



LAIAR-011-009_03 FWH 24/04/2007 Underside of deck.



LAIAR-011-009_04 FWH 24/04/2007 Underside of deck, looking west.



LAIAR-011-009_05 FWH 24/04/2007 Detailing of principal girder.



LAIAR-011-009_06 FWH 24/04/2007 Maker's plaque on downstream side.



LAIAR-011-009_07 FWH 24/04/2007 Downstream (SE) face of flood arch.



Site no **LAIAR-011-011** **Roundwood Bridge**

County Laois **Townland** Killinure (Upperwoods By);
Paddock **Town**

Planning Laois CC **Discovery map** 54 **Six-inch map** 11

Summary A triple-arch masonry road bridge of 18th/early 19th century date over the Mountrath River.

History Roundwood Bridge is cited on the 1838, 1888 and 1907 OS maps.

Component **LAIAR-011-011 1**

Type Bridge (road/river) Function Road & pedestrian

Category Transport Context Infrastructure

Irish Grid 232361 197534 +/- 10m

Survey date 24/04/2007 Surveyor Fred Hamond

Remains Complete Condition Good Current use Road bridge

Description A triple-span masonry bridge carries a public road over the Mountrath River. The abutments and piers are of dressed masonry blocks laid to regular courses. There are similarly detailed triangular cutwaters to the upstream (north) ends of the piers, rising to arch spring level. Only the middle arch is in normal use. The left-bank arch is skewed, in line with the angle of the road at this end of the bridge. The arches are of semi-elliptical profile. Their voussoirs are of dressed, vee-jointed stone. Between every alternate voussoir stone are three smaller stones, arranged such that the two outer ones are laid over the inner one. From the left to right bank, the arch spans are 4.48m, 4.59m and 4.55m. The spandrels and parapets are of roughly-dressed squared random rubble, set in courses. The parapets, which are 6.41m apart, are coped with split stones laid on their edges. The approaches are dog-legged.

Interest Architectural; Setting

Evaluation This bridge is of architectural interest on account of its stonework, notably the detailing on its voussoirs. It also adds interest to the riverscape hereabouts. Overall, it is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.

Rating Regional **Protection** **Action** RPS

Associated with

Other dbase LA Bridge R149

References

Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002). Page(s) 175

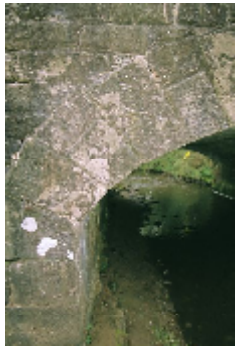
LAIAR-011-011_01 FWH 24/04/2007 Downstream (S) elevation.



LAIAR-011-011_02 FWH 24/04/2007 Looking across downstream face from east bank.



LAIAR-011-011_03 FWH 24/04/2007 Voussoir detail (downstream side of middle arch).



LAIAR-011-011_04 FWH 24/04/2007 Parapet coping (downstream side).



Site no LAIAR-011-017

County Laois **Townland** Bughorn; Dernamanagh **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 11

Summary Site of 18th/early 19th century foot bridge over Delour River.

History A foot bridge over the Delour River is shown on the 1838 and 1888 OS maps. It is not on the 1907 edition.

Component LAIAR-011-017 1

Type Bridge (foot/river) **Function** Road & pedestrian
Category Transport **Context** Infrastructure
Irish Grid 229455 196918 +/- 10m
Survey date 14/06/2007 **Surveyor** Fred Hamond
Remains No visible remains **Condition** N/A **Current use** N/A
Description Site of foot bridge carrying an accommodation track over the Delour River.

Interest None

Evaluation No industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase

References

Site no LAIAR-011-022

County Laois **Townland** Glendine (Upperwoods By) **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 11

Summary A later 20th century pre-cast reinforced-concrete beam and slab road bridge over Killeen River.

History This is a later 20th century replacement of a ford shown on the 1907 OS map.

Component LAIAR-011-022 1

Type Bridge (road/river) **Function** Road & pedestrian
Category Transport **Context** Infrastructure
Irish Grid 224957 199835 +/- 10m
Survey date 24/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge
Description A skewed single-span concrete bridge carries a public road over the Killeen River. The abutments are of concrete. The span comprises pre-cast reinforced-concrete beams with a slab deck over. It spans 3.19m (measured orthogonally; 4.68m on the skew). The parapets are of random rubble and are spaced at 4.37m.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase

References

Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002). Page(s) 118

LAIAR-011-022_01 FWH 24/04/2007 Upstream (N) elevation.



Site no LAIAR-011-032

County Laois **Townland** Paddock **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 11

Summary A later 20th century reinforced-concrete slab accommodation bridge over the Mountrath River.

History Stepping stones are cited on the 1838, 1888 and 1907 OS maps. A ford is also cited on the 1907 edition. The bridge is of later 20th century date. According to Scully, the ford was in use to c.1970.

Component LAIAR-011-032 1

Type Bridge (road/river) **Function** Road & pedestrian
Category Transport **Context** Infrastructure
Irish Grid 233136 196954 +/- 10m
Survey date 24/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge
Description A concrete bridge carries an accommodation road over the Mountrath River. The abutments are of concrete. The deck comprises a reinforced-concrete slab which spans 6.10m and is 4.85m wide. There are falsework sockets along the tops of the inside faces of the abutments. The parapets comprise three tubular steel bars. A pipe is carried along the south-east edge of the deck.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase

References

Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002). Page(s) 175

LAIAR-011-032_01 FWH 24/04/2007 Downstream (SE) elevation.



LAIAR-011-032_02 FWH 24/04/2007 Bird nest in falsework hole under deck.



Site no LAIAR-012-007

Broken Bridge

County Laois **Townland** Ballycormick; Cavansheath **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 12

Summary A masonry arch road bridge of 18th/early 19th century date over a tributary of the Mountrath River.

History A bridge is shown on the 1839, 1889 and 1907 OS maps. Broken Bridge is explicitly cited on the 1907 edition.

Component LAIAR-012-007 1

Type Bridge (road/river) **Function** Road & pedestrian
Category Transport **Context** Infrastructure
Irish Grid 236952 199856 +/- 10m
Survey date 24/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge
Description An arched masonry bridge carries a public road over a tributary of the Mountrath River. It is of random rubble construction throughout. The arch is of segmental profile and spans 2.92m. Its abutments are 8.80m deep. The downstream (SE) parapet is set back slightly from the face of the bridge.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

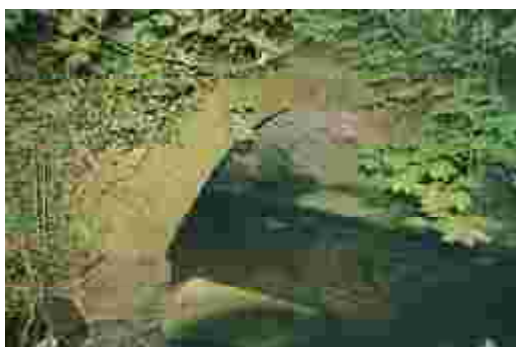
Associated with

Other dbase LA Bridge C044

References

Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002). Page(s) 98

LAIAR-012-007_01 FWH 24/04/2007 Downstream (SE) elevation.



Site no LAIAR-012-009 **Finn's Bridge; Ross Bridge**

County Laois **Townland** Iry; Ross (Maryborough East By); Springfield **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 12

Summary A twin-arched masonry road bridge of 18th/early 19th century date over River Blackwater.

History Ross Bridge is cited on the 1839, 1889 and 1907 OS maps. According to Scully, it is now known as Finn's Bridge after a neighbouring family.

Component LAIAR-012-009 1

Type Bridge (road/river) **Function** Road & pedestrian
Category Transport **Context** Infrastructure
Irish Grid 241316 199727 +/- 10m
Survey date 24/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Fair **Current use** Road bridge

Description A twin-arched masonry bridge carries a public road over the River Blackwater. Only the right-bank arch is normally in use. The bridge is of random rubble construction throughout. There is a triangular concrete cutwater to the upstream (south) pier, rising to arch spring level. The arches are of semicircular profile; the left-bank one spans 2.46m, and the right-bank one 2.48m. The abutments are 8.56m deep. There is a reinforced-concrete strengthening beam through each end of the soffits. The abutments and piers also have concrete footings. The bridge is heavily overgrown. Part of the voussoir on the downstream end of the left-bank arch and adjoining section of the approach wall have collapsed.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase LA Bridge C040

References

Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002). Page(s) 146

LAIAR-012-009_01 FWH 24/04/2007 Downstream (N) elevation.



Site no LAIAR-012-012 **Briscula Bridge**

County Laois **Townland** Briscula; Cavansheath **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 12

Summary Site of a masonry arch road bridge of 18th/early 19th century date over a tributary of the Mountrath River. Replaced in the later 20th century by a reinforced-concrete slab bridge.

History A bridge is shown on the 1839, 1889 and 1907 OS maps. Briscula Bridge is explicitly cited on the 1907 edition.

Component LAIAR-012-012 1

Type Bridge (road/river) Function Road & pedestrian
Category Transport Context Infrastructure
Irish Grid 235887 199075 +/- 10m
Survey date 24/04/2007 Surveyor Fred Hamond
Remains No visible remains Condition N/A Current use N/A
Description No trace of this bridge survives, having been superseded by a concrete one (component 2).

Component LAIAR-012-012 2

Type Bridge (road/river) Function Road & pedestrian
Category Transport Context Infrastructure
Irish Grid 235887 199074 +/- 10m
Survey date 24/04/2007 Surveyor Fred Hamond
Remains Complete Condition Good Current use Road bridge
Description A concrete bridge carries a public road over a tributary of the Mountrath River. The abutments are of concrete. The deck comprises a reinforced-concrete slab which spans 3.79m. The parapets are also of concrete and are spaced at 6.03m. The random rubble approach walls of the original bridge survive.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase LA Bridge C045

References

Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002).

Page(s) 83

LAIAR-012-012_01 FWH 24/04/2007 Upstream (NW) elevation.



Site no **LAIAR-012-013** **Needleford Bridge**

County Laois **Townland** Brockra; Ringstown **Town**
Planning Laois CC **Discovery map** 54 **Six-inch map** 12

Summary A masonry arch road bridge of 18th/early 19th century date over a tributary of the Mountrath River.

History Needleford Bridge is captioned on the 1839, 1889 and 1907 OS maps.

Component **LAIAR-012-013 1**

Type Bridge (road/river) Function Road & pedestrian
Category Transport Context Infrastructure
Irish Grid 236675 197843 +/- 10m
Survey date 14/06/2007 Surveyor Fred Hamond
Remains Complete Condition Good Current use Road bridge
Description A masonry arch bridge carries a road over a tributary of the Mountrath River. It is constructed of random rubble masonry. The arch is of segmental profile and spans 3.58m A pipe is carried through the soffit towards the upstream (E) end. Very overgrown.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase LA Bridge C043

References

Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002). Page(s) 84-85

LAIAR-012-013_01 FWH 14/06/2007 Upstream (E) elevation.



Site no LAIAR-012-017

County	Laois	Townland	Clonboyne	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	12

Summary A skew arched road bridge over Dublin-Cork railway line, opened by Great Southern & Western Railway Co in 1847. The soffit is of skewly-laid brick.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway was opened in 1847. An uncaptioned bridge is marked at this location on the 1889 and 1907 OS maps.

Component LAIAR-012-017 1

Type	Bridge (road/rail)	Function	Railway		
Category	Transport	Context	Infrastructure		
Irish Grid	244300 196644 +/- 10m				
Survey date	14/06/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A skew masonry arch bridge carries a road over the double-track Dublin-Cork line over a public road. The abutments are of rock-faced masonry blocks laid to regular courses and embellished with tooled quoins. There are similarly detailed buttresses to each side. The arch is of semi-elliptical profile and spans 9.23m (9.89m on the skew). A string course runs through the arch at spring level and around the quoins. The arch soffit is of skewly-laid brickwork. The voussoirs are rusticated and have a tooled edge. They are stepped into the spandrels which are detailed as the abutments. The parapets are 6.57m apart and of squared random rubble. They are coped with dressed stone blocks and terminate in out-projecting piers which are continuations of the buttresses. The piers are embellished with rusticated and tooled quoins. A string course runs along the outside base of the parapets and around their piers. Curved wing walls project at each end, detailed as the parapets. The approach roads are ramped and dog-legged. This is Iarnród Éireann bridge no.150.				

Interest Architectural; Historical; Technical

Evaluation This is a good example of a well-executed skew brick and stone bridge and is of architectural and technical merit. It also has historical interest due to its connection with the railway. It is of local industrial heritage significance.

Rating	Local	Protection		Action	None
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Associated with LAIAR-012-016

Other dbase

References

LAIAR-012-017_01 FWH 14/06/2007 SW elevation.



LAIAR-012-017_02 FWH 14/06/2007 Stonework detail.



LAIAR-012-017_03 FWH 14/06/2007 Soffit detail. Note skew-laid bricks.



Site no LAIAR-012-021

County	Laois	Townland	Clonkeen (Maryborough West By)	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	12

Summary A reinforced-concrete beam bridge of c.1964 carries the Dublin-Cork railway line over a Bord na Mona peat railway serving the Coolnamona group of bogs.

History This bridge was erected by Bord na Mona to carry the main Dublin-Cork line over a peat extraction railway serving the Coolnamona group of bogs. It dates to c.1964. The peat is for horticultural use.

Component LAIAR-012-021 1

Type	Bridge (rail/rail)	Function	Peat		
Category	Mineral extraction, processing & products	Context	Industry		
Irish Grid	243888 196331	+/- 10m			
Survey date	14/06/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Rail bridge
Description	A concrete beam bridge carries the double-track Dublin-Cork railway over a single-track peat extraction railway. It comprises eight pre-cast reinforced-concrete units of inverted U or box profile. The overall length of the abutments is 14.24m and the span is 4.33m. The parapets are four-bar tubular steel railings set in from the ends of the bridge.				

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with LAIAR-012-016; LAIAR-012-020

Other dbase

References

Doyle, O. and Hirsch S. Railway Lines of Coras Iompair Eireann and Northern Ireland Railways (Malahide: Signal Press, 1985).	Page(s)	14
Johnson, S. Johnson's Atlas & Gazetteer of the Railways of Ireland (Leicester: Midland Publishing, 1997).	Page(s)	72

LAIAR-012-021_01 FWH 14/06/2007 SE elevation.



LAIAR-012-021_02 FWH 14/06/2007 Loco about to pass through bridge.



LAIAR-012-021_03 FWH 14/06/2007 Peat wagons passing through bridge.



LAIAR-012-021_04 FWH 14/06/2007 Pre-cast concrete units.



Site no LAIAR-012-025

County	Laois	Townland	Cappanacleara	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	12

Summary A mid 20th century beam and slab bridge carries an accommodation track over a tributary of the Mountrath River. It replaces an earlier ford.

History A ford over a tributary of the Mountrath River is shown on the 1839, 1889 and 1907 OS maps. This bridge dates from the mid 20th century.

Component LAIAR-012-025 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	236653 197919	+/- 10m			
Survey date	14/06/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A single-span metal beam bridge carries an accommodation track over a tributary of the Mountrath River. The abutments and parapets are of mass concrete. The deck spans 3.20m and comprises a concrete slab over steel I beams. The parapets are 3.33m apart. The ford has been abandoned.				

Interest None

Evaluation No special industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with

Other dbase

References

Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002). Page(s) 92

LAIAR-012-025_01 FWH 14/06/2007 Upstream (N) elevation.



Site no LAIAR-012-031

County	Laois	Townland	Ringstown; Sconce Lower	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	12

Summary A mid 20th century twin-span metal beam and concrete slab road bridge over a tributary of the Mountrath River. Supersedes an earlier foot bridge and ford.

History A ford is shown on the 1839, 1889 and 1907 OS maps. A foot stick is also marked on the 1889 edition and a foot bridge on the 1907 map. The present bridge is of mid 20th century date.

Component LAIAR-012-031 1

Type	Bridge (foot/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	235667 197328 +/- 10m				
Survey date	14/06/2007	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	Superseded by component 2.				

Component LAIAR-012-031 2

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	235664 197326 +/- 10m				
Survey date	14/06/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A twin-span concrete bridge carries a road over a tributary of the Mountrath River. It is of concrete construction throughout. The deck comprises a reinforced-concrete slab; both spans measure 3.02m. The parapets are 3.63m apart. The bed of the river has been concreted.				

Interest None

Evaluation No special industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with

Other dbase

References

Scully, T. Slieve Bloom from Father to Son (Roscrea: Slieve Bloom Rural Development Society, 2002).

Page(s) 189

LAIAR-012-031_01 FWH 14/06/2007 Upstream (E) elevation.



Site no LAIAR-012-034

County	Laois	Townland	Clonkeen (Maryborough West By)	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	12

Summary A reinforced-concrete road bridge of 1964 over a Bord na Mona peat railway serving the Coolnamona group of bogs.

History This bridge was erected in 1964 by Bord na Mona as part of the development of the Coolnamona group of bogs. The peat is for horticultural use.

Component LAIAR-012-034 1

Type	Bridge (road/rail)	Function	Peat		
Category	Mineral extraction, processing & products	Context	Industry		
Irish Grid	243972 196223		+/- 10m		
Survey date	14/06/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A skew reinforced-concrete bridge carries a road over a Bord na Mona peat extraction railway. It is of reinforced-concrete construction throughout. The slab deck spans 4.27m (4.56m on the skew). The parapets are 6.15m apart. A plaque on the road face of the NW parapet reads: "Bord na Mona/ 1964". This is BnaM bridge no.18-02.				

Interest None

Evaluation No special industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with

Other dbase

References

LAIAR-012-034_01 FWH 14/06/2007 NW elevation.



LAIAR-012-034_02 FWH 14/06/2007 Datestone on NW parapet.



Site no LAIAR-012-035

County	Laois	Townland	Clonkeen (Maryborough West By)	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	12

Summary A reinforced-concrete road bridge of c.1964 over a Bord na Mona peat railway serving the Coolnamona group of bogs. Extended by the National Roads Authority in the early 2000s with pre-cast reinforced-concrete pipe sections.

History This bridge was constructed by Bord na Mona, c.1964 in connection with peat extraction from the Coolnamona group of bogs. The peat is for horticultural use. The bridge was extended in the early 2000s to accommodate the motorway by-pass around Portlaoise.

Component LAIAR-012-035 1

Type	Bridge (road/rail)	Function	Peat		
Category	Mineral extraction, processing & products	Context	Industry		
Irish Grid	243695 196590		+/- 10m		
Survey date	14/06/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge

Description A reinforced-concrete bridge carries the N7 over a Bord na Mona peat extraction railway. It is of reinforced-concrete construction throughout. The slab deck spans 4.27m and is 15.51m deep. It has been widened by 32.5m at its NW end to accommodate a motorway which now runs across the entire bridge. The extended section comprises 13 pre-cast reinforced-concrete units of elliptical section; each unit is 2.5m long and 5.74m maximum width. The original concrete parapet survives on the SE elevation. A timber fence acts as the NW parapet.

Interest Technical

Evaluation This bridge is of some technical interest in demonstrating two types of concrete bridge - the conventional cast in situ slab deck, and the more modern pre-cast elliptical pipe units. However, it is of no special industrial heritage merit.

Rating	Record only	Protection		Action	None
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Associated with

Other dbase NRA Bridge LS-N07-005

References

Industrial Railway Bridge: Eirspan Inspection Report (National Roads Authority).	Page(s)
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LAIAR-012-035_01 FWH 14/06/2007 SE elevation of original bridge.



LAIAR-012-035_02 FWH 14/06/2007 NW elevation of extended section.



LAIAR-012-035_03 FWH 14/06/2007 Pre-cast units in extended section.



Site no LAIAR-013-003

County Laois **Townland** Maryborough **Town** Portlaoise

Planning Laois CC **Discovery map** 54 **Six-inch map** 13

Summary Site of road bridge on Mountmellick Branch of Great Southern & Western Railway, opened in 1885.

History This bridge was built by the Central Ireland Railway Company and opened in 1885. It is shown on the 1889 and 1907 OS maps. It served as the Mountmellick Branch of the Great Southern & Western Railway. The line was closed to passenger services in 1947 and to goods traffic in 1963.

Component LAIAR-013-003 1

Type Bridge (road/rail) **Function** Railway

Category Transport **Context** Infrastructure

Irish Grid 245771 199151 +/- 10m

Survey date 30/08/2007 **Surveyor** Fred Hamond

Remains No visible remains **Condition** N/A **Current use** N/A

Description All traces of this bridge and its ramped and dog-legged approaches have been removed to make way for a new road roundabout.

Interest None

Evaluation No industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with LAIAR-013-001

Other dbase

References

Dunne, S. 'Ballyfin road bridge goes, water stays'. In The Leinster Express, Page(s) 11
12 April 2003.

Site no LAIAR-013-004

Kyle Bridge

County	Laois	Townland	Ballytegan; Gorteen (Maryborough East By); Kyleconhobert	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	13

Summary A triple-span masonry arch road bridge of 18th/early 19th century date over the Triogue River.

History Kyle Bridge is captioned on the 1839, 1889 and 1907 OS maps.

Component LAIAR-013-004 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	246370 200597		+/- 10m		
Survey date	30/08/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge

Description A triple-arch masonry bridge carries a road over the Triogue River. It is of random rubble construction throughout. There are mass-concrete triangular cutwaters to the upstream piers, rising to arch spring level. There are also concrete footings to the abutments and piers. The arches are of semicircular profile; from the left to right bank they span 1.7m, 2.1m and 2.1m. The parapets are 5.62m apart. The upstream one on the left-bank has been rebuilt.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase LA Bridge C028

References

LAIAR-013-004_01 FWH 30/08/2007 Downstream (north) elevation.



Site no LAIAR-013-006

County	Laois	Townland	Straboe	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	13

Summary Arched masonry road bridge over Dublin-Cork railway line, opened by Great Southern & Western Railway Co in 1847.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway was opened in 1847. An uncaptioned bridge is marked on the 1907 OS map (its depiction on the 1889 map is unclear).

Component LAIAR-013-006 1

Type	Bridge (road/rail)	Function	Railway		
Category	Transport	Context	Infrastructure		
Irish Grid	248896 202562		+/- 10m		
Survey date	30/08/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge

Description A single-span masonry arch bridge carries a road over the double-track Dublin-Cork railway. The abutments are of regularly-coursed rock-faced masonry blocks, with margined quoins. The arch is of semi-elliptical profile and spans 9.21m. It has vee-joined rusticated voussoirs (with margined edges to each block) which are stepped into the spandrels. The soffit is of masonry blocks. A string course runs around the quoins and through the arch at spring level. The spandrels are of roughly-dressed squared random rubble set in regular courses. A string course runs over the crown of the arch. The parapets, which are 6.59m apart, are of dressed and squared rubble, randomly laid and coped with out-projecting finely-dressed stone blocks. The wing walls are of roughly-dressed squared random rubble; their copings are as the parapet. This is bridge OBC 134. A concrete block steps lead down to the track at its south-east end. The approaches to the bridge are dog-legged and ramped.

Interest Architectural; Historical

Evaluation This bridge is of architectural interest on account of its quality of construction, stonework detailing and embellishment. It also has a historical association with the Great Southern & Western Railway. It is of local industrial heritage significance.

Rating	Local	Protection		Action	None
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Associated with LAIAR-013-005

Other dbase

References

LAIAR-013-006_01 FWH 30/08/2007 NE elevation.



Site no LAIAR-013-007

County	Laois	Townland	Rathnamanagh	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	13

Summary Arched masonry road bridge over Dublin-Cork railway line, opened by Great Southern & Western Railway Co in 1847.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway was opened in 1847. An uncaptioned bridge is marked on the 1889 and 1907 OS maps.

Component LAIAR-013-007 1

Type	Bridge (road/rail)	Function	Railway		
Category	Transport	Context	Infrastructure		
Irish Grid	248497 200986		+/- 10m		
Survey date	30/08/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge

Description A single-span masonry arch bridge carries a road over the double-track Dublin-Cork railway. The abutments are of regularly-coursed rock-faced masonry blocks, with margined quoins. The arch is of semi-elliptical profile and spans 9.22m. It has vee-joined rusticated voussoirs (with margined edges to each block) which are stepped into the spandrels. The soffit is of masonry blocks. A string course runs around the quoins and through the arch at spring level. The spandrels are of roughly-dressed squared random rubble set in regular courses. A string course runs over the crown of the arch. The parapets, which are 6.74m apart, are of roughly-dressed and squared rubble, randomly laid and coped with out-projecting dressed stone blocks. The wing walls are of dressed squared random rubble; their copings are as the parapet. This is bridge OBC 136. The approaches to the bridge are dog-legged and ramped.

Interest Architectural; Historical

Evaluation This bridge is of architectural interest on account of its quality of construction, stonework detailing and embellishment. It also has a historical association with the Great Southern & Western Railway. It is of local industrial heritage significance.

Rating	Local	Protection		Action	None
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Associated with LAIAR-013-005

Other dbase

References

LAIAR-013-007_01 FWH 30/08/2007 NE elevation.



Site no LAIAR-013-008

County	Laois	Townland	Borris Great	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	13

Summary Arched masonry road bridge over Dublin-Cork railway line, opened by Great Southern & Western Railway Co in 1847.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway was opened in 1847. An uncaptioned bridge is marked on the 1889 and 1907 OS maps.

Component LAIAR-013-008 1

Type	Bridge (road/rail)	Function	Railway		
Category	Transport	Context	Infrastructure		
Irish Grid	248359 200447		+/- 10m		
Survey date	30/08/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge

Description A single-span masonry arch bridge carries a road over the double-track Dublin-Cork railway, here partly in a cutting. The abutments are of regularly-coursed rock-faced masonry blocks, with margined quoins. There are dressed and coursed masonry buttresses to each side. The arch is of semi-elliptical profile and spans 9.12m. It has vee-joined rusticated voussoirs (with a margined edge to the arch) which are stepped into the spandrels. The soffit is of masonry blocks. A string course runs around the quoins and through the arch at spring level. The spandrels are of roughly-dressed squared random rubble set in regular courses. A string course runs over the crown of the arch. The parapets, which are 3.72m apart, are of roughly-dressed and squared rubble, randomly laid and coped with out-projecting dressed stone blocks. The wing walls were too overgrown to describe in detail. This is bridge OBC 138. The approaches to the bridge are dog-legged and ramped.

Interest Architectural; Historical

Evaluation This bridge is of architectural interest on account of its quality of construction, stonework detailing and embellishment. It also has a historical association with the Great Southern & Western Railway. It is of local industrial heritage significance.

Rating	Local	Protection		Action	None
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Associated with LAIAR-013-005

Other dbase

References

LAIAR-013-008_01 FWH 30/08/2007 NE elevation.



Site no LAIAR-013-009

County Laois **Townland** Borris Great **Town** Portlaoise

Planning Laois CC **Discovery map** 54 **Six-inch map** 13

Summary An arched masonry railway bridge over tributary of the Triogue River. On the Dublin-Cork railway line, opened by Great Southern & Western Railway Co in 1847.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway was opened in 1847. An uncaptioned bridge is marked on the 1889 and 1907 OS maps.

Component LAIAR-013-009 1

Type Bridge (rail/river) **Function** Railway

Category Transport **Context** Infrastructure

Irish Grid 247854 199608 +/- 10m

Survey date 11/05/2007 **Surveyor** Fred Hamond

Remains Complete **Condition** Good **Current use** Rail bridge

Description A bridge carries the double-track Dublin-Cork railway over a tributary of the Triogue River. Dense overgrowth precluded inspection, but it appears to be an arched masonry bridge spanning two to three metres.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with LAIAR-013-005

Other dbase

References

Site no LAIAR-013-010

County Laois **Townland** Borris Great; Borris Little **Town** Portlaoise

Planning Laois CC **Discovery map** 54 **Six-inch map** 13

Summary An arched masonry railway bridge over a road, on the Dublin-Cork railway line; opened by Great Southern & Western Railway Co in 1847.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway was opened in 1847. An uncaptioned bridge is marked on the 1889 and 1907 OS maps.

Component LAIAR-013-010 1

Type Bridge (rail/road) **Function** Railway

Category Transport **Context** Infrastructure

Irish Grid 247779 199529 +/- 10m

Survey date 11/05/2007 **Surveyor** Fred Hamond

Remains Complete **Condition** Good **Current use** Rail bridge

Description An arched masonry bridge carries the double-track Dublin-Cork railway over a road. The abutments are of roughly dressed masonry blocks brought to courses. They have rusticated quoins with tooled margins and are 9.23m deep. A string course runs through the arch at spring level and around the quoins. The arch is of segmental profile and spans 3.63m. The voussoirs are of rusticated blocks, each with margined edges. Its soffit is of rock-faced masonry blocks. The spandrels are radial continuations of the voussoirs, with a string course over. The parapets are of coursed and rock-faced masonry blocks and are coped with ashlar. Down-sloping ashlar-coped wing walls, detailed as the abutments, project at right angles and terminate in rusticated piers. A sign on each side of the arch crown reads 9'6", 2.91m. The approach road is dog-legged. This is Iarnród Éireann bridge no.394.

Interest Architectural; Historical

Evaluation This bridge is of architectural interest on account of the quality of its construction and stonework embellishment. It also has a historical association with the Great Southern & Western Railway. It is of local industrial heritage significance.

Rating Local **Protection** **Action** None

Associated with LAIAR-013-005

Other dbase

References

LAIAR-013-010_01 FWH 11/05/2007 SE elevation.



Site no **LAIAR-013-017**

County Laois **Townland** Knockmay **Town** Portlaoise
Planning Laois CC **Discovery map** 54 **Six-inch map** 13

Summary Site of road bridge over Mountmellick Branch of Great Southern & Western Railway, opened in 1885.

History This bridge was built by the Central Ireland Railway Company and opened in 1885. It is shown on the 1888 and 1907 OS maps. It served as the Mountmellick Branch of the Great Southern & Western Railway. The line was closed to passenger services in 1947 and to goods traffic in 1963.

Component **LAIAR-013-017 1**

Type Bridge (road/rail) Function Railway
Category Transport Context Infrastructure
Irish Grid 246093 198319 +/- 10m
Survey date 11/05/2007 Surveyor Fred Hamond
Remains No visible remains Condition N/A Current use N/A
Description No traces of this bridge survive. The OS map show its approaches to have been ramped.

Interest None

Evaluation No industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with LAIAR-013-001

Other dbase

References

Site no LAIAR-013-018

County Laois **Townland** Knockmay **Town** Portlaoise

Planning Laois CC **Discovery map** 54 **Six-inch map** 13

Summary Site of railway bridge carrying the Mountmellick Branch of the Great Southern & Western Railway (opened 1885) under the Dublin-Cork line (opened by Great Southern & Western Railway Co in 1847).

History This bridge was built by the Central Ireland Railway Company and opened in 1885. It is shown on the 1889 and 1907 OS maps and served as the Mountmellick Branch of the Great Southern & Western Railway.

Component LAIAR-013-018 1

Type Bridge (rail/rail) **Function** Railway

Category Transport **Context** Infrastructure

Irish Grid 246195 198068 +/- 10m

Survey date 30/08/2007 **Surveyor** Fred Hamond

Remains No visible remains **Condition** N/A **Current use** N/A

Description There are no visible traces of this bridge, which passed under the main Dublin-Cork line. Although it was probably demolished, there is a possibility that it was infilled so that there are buried remains. The area immediately south of it is now the main Iarnród Éireann sleeper depot.

Interest None

Evaluation No industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with LAIAR-013-001; LAIAR-013-005

Other dbase

References

Site no	LAIAR-013-019	Boghlone Bridge
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County	Laois	Townland	Boghlone; Clonroosk Little; Pallas Big	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	13

Summary Site of an 18th/early 19th century road bridge over tributary of Triogue River. It has been replaced in the later 20th century by a twin-span reinforced-concrete bridge.

History A bridge is shown on the 1839 and 1889 OS maps. It is captioned as Boghlone Bridge on the 1907 edition.

Component LAIAR-013-019 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	245239 198328 +/- 10m				
Survey date	14/06/2007	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	Site of road bridge over tributary of Triogue River.				

Component LAIAR-013-019 2

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Infrastructure		
Irish Grid	245238 198328 +/- 10m				
Survey date	14/06/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A twin-span bridge carries a road over a tributary of the Triogue River. It is of concrete throughout. The middle section incorporates vestiges of the former masonry arch bridge. The two spans of this section, which is 10.3m deep, are of semicircular profile: the left bank arch is 1.53m wide and the right-bank one 1.54m. The upstream side has been widened by 6.1m with a reinforced-concrete slab deck. Its left and right spans are 1.85m and 2.26m respectively. There is a triangular cutwater up the pier. The parapet is of mass concrete. The downstream side has been extended by 6.0m, again with a concrete slab deck. It left span is 1.84m and the right one is 2.04m across. There are falsework holes along the tops of the abutments. The parapet is of concrete blocks. There is an commemorative plaque on the downstream (north) parapet to a road traffic victim.				

Interest	None
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Evaluation	No special industrial heritage significance.
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Rating	Record only	Protection		Action	None
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Associated with

Other dbase

References

LAIAR-013-019_01 FWH 14/06/2007 Downstream (N) elevation.



LAIAR-013-019_02 FWH 14/06/2007 Downstream elevation of middle section of right-bank arch showing vestige of original split rubble masonry arch.



Site no LAIAR-013-021

County	Laois	Townland	Togher	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	13

Summary A masonry arch accommodation bridge over the Portlaoise-Waterford railway, opened by the Kilkenny Junction Railway Co in 1867.

History The bridge is on the Portlaoise-Waterford railway line. This section, from Portlaoise to Abbeyleix, was opened by the Kilkenny Junction Railway Co in 1867. The line was taken over by the Great Southern and Western Railway Co. The bridge is depicted on the 1889 and 1907 OS maps.

Component LAIAR-013-021 1

Type	Bridge (road/rail)	Function	Railway		
Category	Transport	Context	Infrastructure		
Irish Grid	246137 196804 +/- 10m				
Survey date	30/08/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A masonry arch bridge carries a road over the former Portlaoise- Waterford railway. The abutments are of rock-faced masonry blocks laid randomly and embellished with tooled quoins. There are similarly detailed tapered buttresses on each face. The arch is of semi-elliptical profile and spans 8.61m. Its voussoirs are of rusticated masonry blocks with a tooled edge to the arch. Its soffit is of squared masonry blocks. The spandrels and parapets are of roughly-dressed masonry blocks, laid randomly. The parapets are 6.09m apart and are coped with rusticated and margined blocks. Wing walls project at right angles; they are detailed as the abutments and are coped with tooled and rusticated blocks; their ends are also rusticated and margined. The approaches are dog-legged and ramped. Just south-west of the bridge, at the entrance to a farmyard off the road, is a finely-crafted pair of wrought-iron gates hung from slender granite posts.				

Interest Architectural; Historical

Evaluation This bridge is of architectural interest due to the quality of its construction. It is also of historical interest due to its association with the Kilkenny Junction Railway. It is of local industrial heritage merit.

Rating	Local	Protection		Action	None
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Associated with LAIAR-013-020

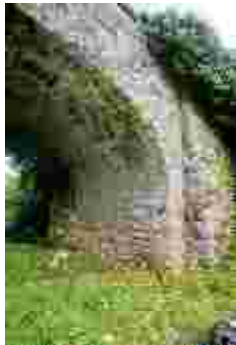
Other dbase

References

LAIAR-013-021_01 FWH 30/08/2007 South elevation.



LAIAR-013-021_02 FWH 30/08/2007 SE abutment showing stone detailing.



Site no LAIAR-013-025

County	Laois	Townland	Knockmay; Maryborough	Town	Portlaoise
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Planning	Laois CC	Discovery map	54	Six-inch map	13
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Summary A skew metal girder railway bridge of 1847 over a road on the Dublin-Cork line. The bridge was widened in 1867 to accommodate a line to Waterford. The decks are 20th century replacements.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway was opened in 1847. An uncaptioned bridge is marked on the 1889 and 1907 OS maps. It was widened around 1867, when the Portlaoise-Waterford railway line was opened by the Kilkenny Junction Railway Company (this bridge is at the north end of the lines' junction).

Component LAIAR-013-025 1

Type	Bridge (rail/road)	Function	Railway
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Category	Transport	Context	Infrastructure
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Irish Grid	246521 198313 +/- 10m
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Survey date	11/05/2007	Surveyor	Fred Hamond
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Remains	Complete	Condition	Good	Current use	Rail bridge
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Description A skew metal span carries the double-track Dublin-Cork line over a public road (and two footpaths) at the west end of Portlaoise. A vertical break in the abutments indicate that the bridge was widened at its east end (to take a single track line to Waterford). The earlier west end comprises rock-faced masonry blocks laid to regular courses and embellished with tooled quoins. The extension is similarly detailed (but without quoin tooling where it abuts the existing abutments). The original abutments are 15.63m deep and the extension is 8.80m. There is a small semicircular opening at ground level on the road face of the extended south abutment which probably gives access to a water pipe. The deck comprises three riveted steel beams with troughing between their bottom flanges; it is undoubtedly a replacement of the original. The additional deck comprises two RSJs with similar troughing; it, too, is probably a replacement. Their overall span is 9.02m as measured orthogonally, and 14.28m on the skew. The beams rest on concrete pillow blocks at the tops of the masonry abutments; they are evidently replacements. The parapets comprise two-bar steel railings on brackets affixed to the outside faces of the outer principal beams. Angled roughly-dressed randomly-laid masonry block wing walls project at each end. At their west end, they terminate in small terminal piers detailed as the abutments. Across each side of the bridge is a sacrificial girder beam, the ends of which are supported on stone-faced concrete blocks set into the tops of the wing walls. They carry height painted timber sheeting and circular warning signs (14ft 1in; 4.30m); both have been hit by lorries. This is Iamród Éireann bridge no.2.

Interest	Architectural; Historical
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Evaluation	This bridge is of historical interest due to its connection with the Great Southern & Western Railway and the fact that its architecture (specifically the abutments) reflects both the original line to Cork and the extension, 20 years later, to Waterford. It is also a landmark on the approach to Portlaoise from the west. It is of local industrial heritage significance.
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Rating Local **Protection** **Action** None

Associated with LAIAR-013-005

Other dbase NIAH 12506059 (L)

References

LAIAR-013-025_01 FWH 11/05/2007 East elevation.



LAIAR-013-025_02 FWH 11/05/2007 Widened section of south abutment. Note wall break with original abutment at right.



LAIAR-013-025_03 FWH 11/05/2007 Traffic impact on sacrificial girder across west end of bridge.



Site no LAIAR-013-027

County Laois **Townland** Maryborough **Town** Portlaoise**Planning** Laois CC **Discovery map** 54 **Six-inch map** 13

Summary A skew metal girder railway bridge of 1847 over a road on the Dublin-Cork line. The bridge was widened in 1867 to accommodate a line to Waterford. The decks are 20th century replacements.**History** This bridge was erected by the Great Southern and Western Railway Company. This section of the railway was opened in 1847. An uncaptioned bridge is marked at this location on the 1889 and 1907 OS maps. It was widened along its south side around 1867, when the Portlaoise-Waterford railway line was opened by the Kilkenny Junction Railway Company (this bridge is at the north end of the lines' junction). There is also an extension on its north side, possibly of around the same date.**Component LAIAR-013-027 1****Type** Bridge (rail/road) **Function** Railway**Category** Transport **Context** Infrastructure**Irish Grid** 246746 198498 +/- 10m**Survey date** 11/05/2007 **Surveyor** Fred Hamond**Remains** Complete **Condition** Good **Current use** Rail bridge**Description** A slightly skew metal span carries the multiple-track Dublin-Cork line over the N80 (and two footpaths). Two vertical breaks in the abutments indicate that the bridge was widened along its NW and SE sides (the latter to take a single track line to Waterford). The original middle section, which is 10.0m deep, comprises coursed rock-faced masonry blocks, with tooled edges to the quoins. The SE section (7.50m deep) comprises randomly-laid rock-faced blocks, with tooled edges to their south quoins. The NW section (4.13m deep) comprises dressed blocks, also randomly laid. There are coursed rock-faced masonry block buttresses to both ends of the SE side. The deck comprises five riveted steel beams with riveted steel troughing between their bottom flanges; it is undoubtedly a replacement. Their overall span is 9.15m as measured orthogonally, and 9.46m on the skew. The deck, which is undoubtedly a replacement, is narrower than the overall 21.63m depth of the abutments. The parapets comprise four-bar steel railings on brackets affixed to the outside faces of the outer principal beams. The railings terminate in dressed and coursed masonry block walls (missing at the SE corner). Dressed and coursed masonry block wing walls project at right angles at each end (but not at the SE corner). The wing walls at NW terminate in small rock-faced piers. Across each side of the bridge is a sacrificial girder beam, the ends of which are supported on stone-faced concrete blocks set into the tops of the wing walls. They carry height painted timber sheeting and circular warning signs (13ft 5in; 4.10m). This is numbered both as Iarnród Éireann bridge no.1 and no.145.**Interest** Architectural; Historical; Setting**Evaluation** This bridge is of historical interest due to its connection with the Great Southern & Western Railway and the fact that its architecture (specifically the abutments) reflects both the original line to Cork and the extension, 20 years later, to Waterford. It is also a landmark on the approach to Portlaoise from Tullamore. It is of local industrial heritage significance.

Rating Local **Protection** **Action** None

Associated with LAIAR-013-005

Other dbase NIAH 12504257 (L); NRA Bridge LS-N80-007

References

Portaloise Railway Bridge: Eirspan Inspection Report (National Roads Authority). Page(s)

LAIAR-013-027_01 FWH 11/05/2007 SE elevation.



LAIAR-013-027_02 FWH 11/05/2007 SW abutment (note wall break) and underside of deck showing riveted troughing.



Site no	LAIAR-013-028	Portlaoise Station
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County	Laois	Townland	Maryborough	Town	Portlaoise
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Planning	Laois CC	Discovery map	54	Six-inch map	13
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Summary Railway station on Dublin-Cork railway line, opened by Great Southern & Western Railway Co in 1847. Site encompasses station and station master's house, waiting room, workshop and two late 19th/early 20th century lattice girder foot bridges (one now gone). The station building also incorporates Victorian letter box. On north side of line are 19th century warehouse, 20th century parcel office and site of cattle pens. Also terminus of Kilkenny & Mountmellick Railway.

History This bridge, at the SW end of the station building, was erected by the Great Southern and Western Railway Company. It is first shown on the 1907 OS map and is therefore of late 19th/early 20th century date. It was manufactured by Edward Manisty, Dundalk. Old photographs show there to have been a second lattice girder foot bridge at the NE end of the station building. It is not marked on the OS maps.

Component LAIAR-013-028 4

Type	Bridge (foot/rail)	Function	Railway		
Category	Transport	Context	Infrastructure		
Irish Grid	246898 198653	+/- 10m			
Survey date	11/05/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Foot bridge
Description	A single-span metal lattice girder foot bridge over the double-track railway line at Portlaoise Station. The span comprises T-section top and bottom strings between which is lattice bracing. The girders are also braced to their outside faces with curved metal brackets from the underside of the deck. Metal plates are laid across the bottom members to form the deck. At each end of the deck are cast-iron steps down to quarter-turn half-landings supported on columns, from which more steps lead down to the platforms. The supporting columns at the NW end are the cast-iron originals and bear their maker's name 'E Manisty & Co Ltd Dundalk'. The columns at the other end are later replacements in steel. The staircase at the NW end also has round newel posts with acorn finials. The free span is 14.05m and the deck is 1.38m wide. Holes have been drilled in the deck plates to facilitate drainage. This is Iarnród Éireann bridge no.144. Although the bridge is still used, there is a modern one (with lift) to its immediate SW.				

Component LAIAR-013-028 5

Type	Bridge (foot/rail)	Function	Railway		
Category	Transport	Context	Infrastructure		
Irish Grid	246921 198673	+/- 10m			
Survey date	11/05/2007	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	No traces of this bridge, at the NE end of the station building, survive.				

Interest	Architectural; Group; Historical; Rarity; Setting; Technical		
Evaluation	The surviving foot bridge is primarily of architectural, historical and technical interest as a typical, but now rare, example of a Great Southern & Western station lattice girder foot bridge. Enhances interest of station and also has group value with rest of complex. It is of regional industrial heritage significance and merits its present inclusion in Record of Protected Structures (LA 471)		
Rating	Regional	Protection	RPS LA 176 (station); RPS LA 464 (house); RPS LA 471 (foot bridge); RPS LA 472 (waiting room)
		Action	None (in RPS)

Associated with

Other dbase NIAH 12504129 (R); NIAH 12504130 (RO); NIAH 12504131 (R); NIAH 12504133 (L); NIAH 12504134 (L); NIAH 12504135 (RO); NIAH 12504330 (R); NIAH 12504338 (R); NIAH 12504341 (L)

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LAIAR-013-028_01 FWH 11/05/2007 General view from NE.



LAIAR-013-028_02 FWH 11/05/2007 Lattice girder and bracket detailing.



LAIAR-013-028_03 FWH 11/05/2007 Staircase at NW end; note newel posts with finials.



Site no LAIAR-013-029

County Laois **Townland** Maryborough **Town** Portlaoise**Planning** Laois CC **Discovery map** 54 **Six-inch map** 13

Summary A slightly skew masonry arch bridge carries the Dublin-Cork railway over the Triogue River. This section of line was opened by Great Southern & Western Railway Co in 1847.**History** This bridge was erected by the Great Southern and Western Railway Company. This section of the railway was opened in 1847. An uncaptioned bridge is marked on the 1889 and 1907 OS maps.

Component LAIAR-013-029 1**Type** Bridge (rail/river) **Function** Railway**Category** Transport **Context** Infrastructure**Irish Grid** 247012 198767 +/- 10m**Survey date** 11/05/2007 **Surveyor** Fred Hamond**Remains** Complete **Condition** Good **Current use** Rail bridge**Description** A slightly skew arched masonry bridge carries the double-track Dublin-Cork railway over the Triogue River. The abutments are of roughly-dressed masonry blocks, brought to courses and 23.95m deep. A string course runs through the arch at spring level and around the quoins. The arch is of semicircular profile and spans 3.68m orthogonally, and 3.89m on the skew. Its voussoirs are of rusticated blocks, each with margined edges. The spandrels are of dressed and coursed masonry blocks, with a string course over the arch crown. Randomly laid masonry block wing walls project at right angles on all but the NE end. The actual line is carried on an earthen embankment above the bridge. A modern cantilevered metal footpath runs through the arch along the left-bank (SW) abutment; it is partly supported on a concrete footing to this abutment.**Interest** Architectural; Historical**Evaluation** This bridge is of architectural interest on account of the quality of construction, stonework detailing and depth. It also has a historical association with the Great Southern & Western Railway. It is of local industrial heritage significance.**Rating** Local **Protection** **Action** None

Associated with**Other dbase** NIAH 12504109 (L)

References

LAIAR-013-029_01 FWH 11/05/2007 Upstream (SE) elevation.



Site no LAIAR-013-030

County Laois **Townland** Borris Little; Maryborough **Town** Portlaoise

Planning Laois CC **Discovery map** 54 **Six-inch map** 13

Summary A troughed metal railway bridge over a road. The line was opened by the Great Southern & Western Railway Co in 1847. The deck is a 20th century replacement.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway was opened in 1847. An uncaptioned bridge is marked on the 1889 and 1907 OS maps. The present deck is a 20th century replacement.

Component LAIAR-013-030 1

Type Bridge (rail/road) **Function** Railway

Category Transport **Context** Infrastructure

Irish Grid 247081 198833 +/- 10m

Survey date 11/05/2007 **Surveyor** Fred Hamond

Remains Complete **Condition** Good **Current use** Rail bridge

Description A single-span metal girder bridge carries the double-track Dublin-Cork railway over a public road. There is also a footpath along the north-east side of the road. The abutments are of regularly-coursed rock-faced masonry blocks, with margined quoins. The replacement span comprises riveted longitudinal metal troughing, on concrete pillow blocks set into the tops of the abutments; they span 7.0m. The parapets comprises two-bar metal railings. At NW, the railing continues as a coursed masonry block parapet which terminates in rock-faced piers. Wing walls project at right angles; these are of dressed and randomly laid masonry blocks. The NW wing wall has been raised in random rubble. A sign on each side of the deck reads 14' 2", 4.34m. This is Iarnród Éireann bridge no.533.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with LAIAR-013-005

Other dbase NIAH 12504310 (L)

References

LAIAR-013-030_01 FWH 11/05/2007 SE elevation.



Site no LAIAR-013-035

County Laois **Townland** Beladd; Borris Little **Town** Portlaoise**Planning** Laois CC **Discovery map** 54 **Six-inch map** 13

Summary Site of early 19th century aqueduct carrying water supply to the former Maryborough Lunatic Asylum over tributary of Triogue river.**History** An aqueduct is captioned on the 1839, 1889 and 1907 OS maps. It carried a water supply to the County Infirmary over a tributary of the Triogue River.

Component LAIAR-013-035 1

Type Bridge (river/river) Function Road & pedestrian

Category Transport Context Infrastructure

Irish Grid 247831 198274 +/- 10m

Survey date 11/05/2007 Surveyor Fred Hamond

Remains No visible remains Condition N/A Current use N/A

Description No trace of this aqueduct survives.

Interest None**Evaluation** No industrial heritage significance.**Rating** Record only **Protection** **Action** None

Associated with**Other dbase**

References

Site no LAIAR-013-041

County Laois **Townland** Borris Great **Town** Portlaoise

Planning Laois CC **Discovery map** 54 **Six-inch map** 13

Summary An masonry arch railway bridge over an accommodation track. On the Dublin-Cork railway line, opened by Great Southern & Western Railway Co in 1847.

History This bridge was erected by the Great Southern and Western Railway Company. This section of the railway was opened in 1847. An uncaptioned bridge is marked on the 1889 and 1907 OS maps.

Component LAIAR-013-041 1

Type Bridge (rail/road) **Function** Railway

Category Transport **Context** Infrastructure

Irish Grid 247904 199656 +/- 10m

Survey date 11/05/2007 **Surveyor** Fred Hamond

Remains Complete **Condition** Good **Current use** Rail bridge

Description An arched masonry bridge carries the double-track Dublin-Cork railway over an accommodation track. The abutments, which are 11.06m deep, are of roughly dressed and squared masonry rubble, laid randomly. The arch is of semicircular profile and spans 3.04m. The voussoirs are of rusticated blocks, each with margined edges. Its soffit is of rock-faced masonry blocks. The spandrels are detailed as the abutments, but brought to courses. No parapets. Down-sloping ashlar-coped wing walls, detailed as the abutments, project at right angles and terminate in small square piers. Immediately SW of the bridge is a small semicircular arched drainage culvert (now dry).

Interest Architectural; Historical

Evaluation This bridge is of architectural interest on account of the quality of construction. It also has a historical association with the Great Southern & Western Railway. It is of local industrial heritage significance.

Rating Local **Protection** **Action** None

Associated with LAIAR-013-005

Other dbase

References

LAIAR-013-041_01 FWH 11/05/2007 NW elevation.



LAIAR-013-041_02 FWH 11/05/2007 Rock-faced soffit detailing.



Site no LAIAR-013-058

County Laois **Townland** Maryborough **Town** Portlaoise**Planning** Laois CC **Discovery map** 54 **Six-inch map** 13

Summary Site of 18th/early 19th century road bridge over Triogue River. Superseded by a concrete slab bridge in mid 1900s.**History** A bridge is shown on the 1839, 1889 and 1907 OS maps. The present bridge is a mid 20th century replacement which incorporates one of the original (rebuilt) rubble stone parapets.

Component LAIAR-013-058 1

Type Bridge (road/river) Function Road & pedestrian

Category Transport Context Infrastructure

Irish Grid 247192 198614 +/- 10m

Survey date 11/05/2007 Surveyor Fred Hamond

Remains No visible remains Condition N/A Current use N/A

Description No traces of bridge survive; superseded by component 2.

Component LAIAR-013-058 2

Type Bridge (road/river) Function Road & pedestrian

Category Transport Context Infrastructure

Irish Grid 247192 198615 +/- 10m

Survey date 11/05/2007 Surveyor Fred Hamond

Remains Complete Condition Good Current use Road bridge

Description A single-span concrete bridge carries a public road over the Triogue River. The abutments are of concrete. The deck comprises a reinforced-concrete slab which spans 5.10m. The parapets are 11.23m apart. That on the upstream (SE) side is of random rubble, and the opposite one is of concrete blocks; both are coped with concrete. A small pipe is carried across the upstream face.

Interest None**Evaluation** No special industrial heritage significance.**Rating** Record only **Protection** **Action** None

Associated with**Other dbase** NIAH 12505178 (RO)

References

LAIAR-013-058_01 FWH 11/05/2007 Upstream (SE) elevation.



Site no LAIAR-013-059

County Laois **Townland** Maryborough **Town** Portlaoise

Planning Laois CC **Discovery map** 54 **Six-inch map** 13

Summary Site of an accommodation foot bridge over Triogue River in grounds of Presentation Convent.

History A bridge is shown on the 1839, 1889 and 1907 OS maps. It is within the grounds of Presentation Convent.

Component LAIAR-013-059 1

Type Bridge (foot/river) Function Road & pedestrian

Category Transport Context Settlement

Irish Grid 247248 198561 +/- 10m

Survey date 11/05/2007 Surveyor Fred Hamond

Remains No visible remains Condition N/A Current use N/A

Description Site of an accommodation foot bridge over the Triogue River. The only indication of its former presence is an infilled gap in a rubble stone wall on the left (west) bank and the footpath down to the river on the right bank. A new boundary wall to the convent grounds (within which the bridge lies) has been erected immediately downstream, spanning the river on a reinforced-concrete beam.

Interest None

Evaluation No industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase NIAH 12505177 (L)

References

Site no LAIAR-013-060

County Laois **Townland** Maryborough **Town** Portlaoise

Planning Laois CC **Discovery map** 54 **Six-inch map** 13

Summary Site of accommodation foot bridge over Triogue River in grounds of Presentation Convent.

History A foot bridge is shown on the 1839 and 1888 OS maps. It is captioned on the 1907 edition. Located in the grounds of Presentation Convent.

Component LAIAR-013-060 1

Type Bridge (foot/river) Function Road & pedestrian

Category Transport Context Settlement

Irish Grid 247282 198515 +/- 10m

Survey date 11/05/2007 Surveyor Fred Hamond

Remains No visible remains Condition N/A Current use N/A

Description Site of accommodation foot bridge over Triogue River.

Interest None

Evaluation No industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase NIAH 12505180 (L)

References

Site no LAIAR-013-061

County Laois **Townland** Maryborough **Town** Portlaoise**Planning** Laois CC **Discovery map** 54 **Six-inch map** 13

Summary Site of accommodation foot bridge over Triogue River in grounds of Presentation Convent. Superseded by a concrete bridge in later 1900s.**History** A foot bridge is shown on the 1839 and 1889 OS maps and explicitly cited on the 1907 edition. In the grounds of Presentation Convent. The present bridge is a later 20th century replacement.

Component LAIAR-013-061 1

Type Bridge (foot/river) Function Road & pedestrian

Category Transport Context Settlement

Irish Grid 247288 198505 +/- 10m

Survey date 11/05/2007 Surveyor Fred Hamond

Remains No visible remains Condition N/A Current use N/A

Description Site of accommodation foot bridge over Triogue River. Superseded by component 2.

Component LAIAR-013-061 2

Type Bridge (foot/river) Function Road & pedestrian

Category Transport Context Settlement

Irish Grid 247288 198505 +/- 10m

Survey date 11/05/2007 Surveyor Fred Hamond

Remains Complete Condition Good Current use Foot bridge

Description A concrete foot bridge carries an accommodation footpath over the Triogue River in grounds of Presentation Convent. The left bank (west) abutment is of random rubble and the right-bank one of concrete. The deck is a reinforced-concrete slab spanning 6.03m. The parapets are 1.63m apart. The upstream (SE) one is of random rubble and the opposite one comprises steel railings on a dwarf concrete wall.

Interest None**Evaluation** No special industrial heritage significance.**Rating** Record only **Protection** **Action** None

Associated with**Other dbase** NIAH 12505187 (L)

References

LAIAR-013-061_01 FWH 11/05/2007 Downstream (NW) elevation.



Site no LAIAR-013-062

County Laois **Townland** Maryborough **Town** Portlaoise

Planning Laois CC **Discovery map** 54 **Six-inch map** 13

Summary A reinforced-concrete accommodation road bridge of later 20th century date which gives access to Presentation School.

History Erected in later 1900s to give vehicular access to Presentation School.

Component LAIAR-013-062 1

Type Bridge (road/river) Function Road & pedestrian

Category Transport Context Infrastructure

Irish Grid 247308 198477 +/- 10m

Survey date 11/05/2007 Surveyor Fred Hamond

Remains Complete Condition Good Current use Road bridge

Description A single-span concrete bridge carries an accommodation road over the Triogue River. The left bank (west) abutment is of concrete and the right-bank one of random rubble. The deck is a reinforced-concrete slab spanning 5.90m. The parapets comprise steel railings on dwarf concrete walls; they are 4.15m apart.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase NIAH 12505179 (L)

References

LAIAR-013-062_01 FWH 11/05/2007 Upstream (SE) elevation.



Site no LAIAR-013-063

County Laois **Townland** Maryborough **Town** Portlaoise**Planning** Laois CC **Discovery map** 54 **Six-inch map** 13

Summary Site of an 18th/early 19th century road bridge over Triogue River. Replaced in later 1900s by a skew twin-span concrete slab bridge.**History** A bridge is first shown on the 1907 OS map. The present bridge is a later 1900s century replacement.

Component LAIAR-013-063 1**Type** Bridge (road/river) **Function** Road & pedestrian**Category** Transport **Context** Infrastructure**Irish Grid** 247365 198403 +/- 10m**Survey date** 11/05/2007 **Surveyor** Fred Hamond**Remains** No visible remains **Condition** N/A **Current use** N/A**Description** No trace of bridge. Superseded by component 2.**Component LAIAR-013-063 2****Type** Bridge (road/river) **Function** Road & pedestrian**Category** Transport **Context** Infrastructure**Irish Grid** 247364 198403 +/- 10m**Survey date** 11/05/2007 **Surveyor** Fred Hamond**Remains** Complete **Condition** Good **Current use** Road bridge**Description** A twin-span concrete bridge carries an accommodation road over the Triogue River. The abutments and pier are of concrete and the deck is a reinforced-concrete slab, without parapets. The left-bank (SW) span is 0.70m and the right-hand one 3.28m, both measured orthogonally. The deck is 2.80m wide.

Interest None**Evaluation** No special industrial heritage significance.**Rating** Record only **Protection** **Action** None

Associated with**Other dbase** NIAH 12505190 (RO)

References

LAIAR-013-063_01 FWH 11/05/2007 Upstream (SE) elevation.



Site no LAIAR-013-066

County Laois **Townland** Beladd; Maryborough **Town** Portlaoise**Planning** Laois CC **Discovery map** 54 **Six-inch map** 13

Summary A skew arched masonry bridge of 18th/early 19th century date carries a public road over a tributary of the Triogue River.**History** This bridge is shown on the 1839, 1889 and 1907 OS maps. Now the responsibility of the National Roads Authority.

Component LAIAR-013-066 1

Type Bridge (road/river) Function Road & pedestrian

Category Transport Context Infrastructure

Irish Grid 247853 198424 +/- 10m

Survey date 11/05/2007 Surveyor Fred Hamond

Remains Complete Condition Good Current use Road bridge

Description A skew masonry arch bridge carries the N80 over a tributary of the Triogue River. The watercourse is culverted under the grounds of the former Maryborough County Infirmary on the north side of the bridge, so only the bridge's south face is visible. It is of random rubble construction, with a dressed masonry semicircular arch of c.2.5 span (direct measurement was precluded by a steel trash rack in front of the arch; the NRA record gives it as 2.42m). The high parapets are 12.0m apart. The south one is of concrete blockwork, faced to its road side with random rubble. The abutments have been widened by 1.8m on the south side in concrete and a reinforced-concrete beam placed over them as a fixing for the trash rack.

Interest None**Evaluation** No special industrial heritage significance.**Rating** Record only **Protection** **Action** None

Associated with**Other dbase** NIAH 12505202 (L); NRA Bridge LS-N80-004

References

Masonry Arch Bridge, Portlaoise: Eirspan Inspection Report (National Roads Authority, 2002). Page(s)

LAIAR-013-066_01 FWH 11/05/2007 Upstream (S) elevation.



Site no LAIAR-013-068

County Laois **Townland** Kylekiproe **Town** Portlaoise**Planning** Laois CC **Discovery map** 54 **Six-inch map** 13

Summary A late 1900s steel foot bridge over the Portlaoise Relief Road.**History** This bridge, over the Portlaoise Relief Road, dates from the later 1900s. It connects a shopping centre with the town centre.

Component LAIAR-013-068 1**Type** Bridge (foot/road) **Function** Road & pedestrian**Category** Transport **Context** Infrastructure**Irish Grid** 247247 198215 +/- 10m**Survey date** 11/05/2007 **Surveyor** Fred Hamond**Remains** Complete **Condition** Good **Current use** Foot bridge**Description** A metal beam girder bridge carries a public footpath over the N80 Portlaoise by-pass. The span comprises two RSJs (54cm x 20cm section) at 2.40m centres. Laid between them are smaller steel transoms which carry the concrete deck, along each side of which is a steel mesh handrail. The ends of the span are each supported on two RSJ posts. The free span is 19.97m. Long ramps of similar construction lead up to each end of the deck on its west side; there are two intermediate landings per ramp. There is also a set of traffic lights just west of the bridge which also enable pedestrians to cross the road.

Interest None**Evaluation** No special industrial heritage significance.**Rating** Record only **Protection** **Action** None

Associated with**Other dbase** NIAH 12506139 (RO); NIAH 12507139 (RO); NRA Bridge LS-N80-006

References

Pedestrian Footbridge, Portlaoise: Eirspan Inspection Report (National Roads Authority, 2002). Page(s)

LAIAR-013-068_01 FWH 11/05/2007 Span from NW.



LAIAR-013-068_02 FWH 11/05/2007 Access ramp on west side.



Site no LAIAR-013-069

County Laois **Townland** Maryborough **Town** Portlaoise**Planning** Laois CC **Discovery map** 54 **Six-inch map** 13

Summary A skew reinforced-concrete road bridge of later 20th century date over the Triogue River. Superseded an 18th/early 19th century bridge.**History** A bridge is shown on the 1839, 1889 and 1907 OS maps. The present bridge is a later 20th century replacement.

Component LAIAR-013-069 1

Type Bridge (road/river) Function Road & pedestrian

Category Transport Context Infrastructure

Irish Grid 247375 198385 +/- 10m

Survey date 11/05/2007 Surveyor Fred Hamond

Remains No visible remains Condition N/A Current use N/A

Description No trace of bridge; superseded by component 2.

Component LAIAR-013-069 2

Type Bridge (road/river) Function Road & pedestrian

Category Transport Context Infrastructure

Irish Grid 247375 198385 +/- 10m

Survey date 11/05/2007 Surveyor Fred Hamond

Remains Complete Condition Good Current use Road bridge

Description A slightly skew concrete bridge carries a public road over the Triogue River at the east end of Portlaoise. The abutments are of concrete and the deck is a reinforced-concrete slab of 4.59m span (measured orthogonally). The upstream parapet comprises a steel railing. The downstream one is of random rubble, rendered to its outside face and coped with chamfered blocks (probably reutilized stonework from the original). The parapets are c.21m apart. A pipe is carried across the downstream side. Just upstream is a galvanised metal foot bridge into a car park.

Interest None**Evaluation** No special industrial heritage significance.**Rating** Record only **Protection** **Action** None

Associated with**Other dbase** NIAH 12507140 (L)

References

LAIAR-013-069_01 FWH 11/05/2007 Upstream (SE) elevation.



LAIAR-013-069_02 FWH 11/05/2007 Downstream (NW) elevation.



Site no LAIAR-013-070

County Laois **Townland** Borris Little; Kylekiproe **Town** Portlaoise

Planning Laois CC **Discovery map** 54 **Six-inch map** 13

Summary A reinforced-concrete road bridge of later 1900s date carries the Portlaoise by-pass over the Triogue River.

History This bridge is part of the later 1900s Portlaoise by-pass road. Now the responsibility of the National Roads Authority.

Component LAIAR-013-070 1

Type Bridge (road/river) Function Road & pedestrian

Category Transport Context Infrastructure

Irish Grid 247402 198294 +/- 10m

Survey date 11/05/2007 Surveyor Fred Hamond

Remains Complete Condition Good Current use Road bridge

Description This concrete road bridge carries the N80 Portlaoise by-pass over the Triogue River. The abutments are of concrete. The slab comprises a reinforced-concrete slab of 4.75m span. The parapets are of concrete blockwork. The downstream (north) one has been hit and is now cracked and partly displaced.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase NIAH 12507142 (RO); NRA Bridge LS-N80-005

References

Triogue River Bridge: Eirspan Inspection Report (National Roads Authority, 2002). Page(s)

LAIAR-013-070_01 FWH 11/05/2007 Downstream (N) elevation.



Site no LAIAR-013-071

County Laois **Townland** Borris Little; Kylekiproe **Town** Portlaoise

Planning Laois CC **Discovery map** 54 **Six-inch map** 13

Summary Later 20th century concrete road bridge over Triogue River.

History This bridge dates from the later 1900s.

Component LAIAR-013-071 1

Type Bridge (road/river) **Function** Road & pedestrian

Category Transport **Context** Infrastructure

Irish Grid 247429 198221 +/- 10m

Survey date 11/05/2007 **Surveyor** Fred Hamond

Remains Complete **Condition** Good **Current use** Road bridge

Description A single-span concrete bridge carries a road over the Triogue River into a housing estate. The abutments are of concrete. The span comprises a reinforced-concrete slab of 5.37m span. Its middle section is cantilevered out to either side. The parapets comprise moulded cast concrete balusters on a dwarf concrete wall and all coped with concrete. They terminate in square concrete piers. The parapets are 5.28m apart.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase NIAH 12507137 (RO)

References

LAIAR-013-071_01 FWH 11/05/2007 Upstream (S) elevation.



Site no LAIAR-013-076

County	Laois	Townland	Maryborough	Town	
Planning	Laois CC	Discovery map	54	Six-inch map	12

Summary A reinforced-concrete accommodation road bridge of late 20th century date over the Triogue River.

History Late 20th century accommodation bridge to public car park.

Component LAIAR-013-076 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Settlement		
Irish Grid	247239 198570		+/- 10m		
Survey date	11/05/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge
Description	A single-span concrete bridge carries an accommodation road over the Triogue River. The abutments are of concrete. The deck is a reinforced-concrete slab spanning 4.96m. The concrete parapets are 7.00m apart.				

Interest None

Evaluation No special industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with

Other dbase

References

LAIAR-013-076_01 FWH 11/05/2007 Upstream (SE) elevation.



Site no **LAIAR-014-005** **Grattan Aqueduct**

County Laois **Townland** Courtwood; Rosnamullane **Town**
Planning Laois CC **Discovery map** 55 **Six-inch map** 14

Summary A twin-arched masonry aqueduct of 1790 carries the Athy Branch of the Grand Canal over the Glasha River.

History This stretch of the Athy Branch of the Grand Canal, between Monasterevin and Athy, was constructed by the Grand Canal Company in 1785-91. A plaque notes the aqueduct's name (spelt Gratton), date of construction in 1790 and name of the canal's engineer Richard Evans. 'Grattan Aqueduct' is captioned on the 1839, 1889 and 1907 OS maps.

Component **LAIAR-014-005 1**

Type Bridge (canal/river) **Function** Inland waterway
Category Transport **Context** Infrastructure
Irish Grid 261421 202756 +/- 10m
Survey date 23/04/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Aqueduct

Description A twin-arched masonry bridge carries the Athy Branch of the Grand Canal over the Glasha River. The abutments and piers were buried under silt and not visible. Both arches are of segmental profile and span 4.44m; their soffits are of masonry blocks. Their voussoirs are of dressed and vee-jointed masonry and project slightly beyond the spandrels; both have keystones. There are circular cutwaters to both ends of the pier, rising to arch spring level. Again, their detailing was obscured by silting. The spandrels and parapets are of dressed and coursed stone blocks. The parapets are delineated by a string course. They curve out slightly at each end (but are without terminal piers) and are coped with concrete. Although they are 11.97m apart, the canal is only 4.63m wide on the aqueduct and there is a towpath along both sides (that on the east side is a metalled road). On the canal face of the west parapet is a dressed stone bearing the inscription "Gratton/ Aqueduct/ Richd. Evans/ Engineer/ 1790".

Interest Architectural; Historical; Setting

Evaluation This bridge is of architectural interest on account of its quality of construction and scale. It is also of historical interest due to its association with the Grand Canal and attested date. It also enhances the landscape hereabouts. It is of regional industrial heritage significance and merits its current inclusion in the Record of Protected Structures (LA 593).

Rating Regional **Protection** RPS LA 593 **Action** None (in RPS)

Associated with LAIAR-014-004

Other dbase NIAH 12801402 (R)

References

Craig, M. and Garner, W. Second Report on Areas and Sites of Historical Interest in County Laois (Dublin: An Foras Forbartha, 1976). Page(s) 89

Photographic Collection, Laois County Library (Local Studies Dept), Portlaoise. Page(s)

Survey of the Architectural Heritage of County Laois (Dublin: NIAH, 2002). Page(s)

Waterways Ireland. Grand Canal Barrow Line: Grattan Aqueduct - Inventory and Condition Survey (Collins Engineers, 2007). Page(s)

LAIAR-014-005_01 FWH 23/04/2007 Downstream (E) elevation.



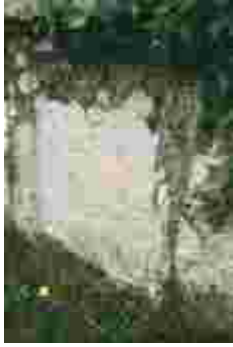
LAIAR-014-005_02 FWH 23/04/2007 Downstream left-bank arch detail.



LAIAR-014-005_03 FWH 23/04/2007 Looking south over aqueduct.



LAIAR-014-005_04 FWH 23/04/2007 Plaque on east parapet.



Site no LAIAR-014-006

County	Laois	Townland	Vicarstown (Dodd)	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	14

Summary An arched masonry culvert of c.1790 carries a tributary of the River Barrow under the Athy Branch of the Grand Canal.

History This stretch of the Athy Branch of the Grand Canal, between Monasterevin and Athy, was constructed by the Grand Canal Company in 1785-91. This aqueduct is captioned on the 1839, 1888 and 1907 OS maps.

Component LAIAR-014-006 1

Type	Bridge (canal/river)	Function	Inland waterway		
Category	Transport	Context	Infrastructure		
Irish Grid	261540 201076		+/- 10m		
Survey date	23/04/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Culvert
Description	An arched masonry culvert carries a tributary of the River Barrow under the Athy Branch of the Grand Canal. Dense vegetation precluded a full survey. The arch is of segmental span and has dressed stone voussoirs; it was estimated to be 1.52m wide.				

Interest Historical

Evaluation Of historical interest due to its association with the Athy Branch of the Grand Canal, this culvert is of local industrial heritage significance.

Rating	Local	Protection		Action	None
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Associated with LAIAR-014-004

Other dbase

References

Site no LAIAR-014-008 **Vicarstown Bridge**

County Laois **Townland** Vicarstown (Cosby);
Vicarstown (Dodd) **Town**

Planning Laois CC **Discovery map** 55 **Six-inch map** 14

Summary An arched masonry road bridge, built c.1790, over the Athy Branch of the Grand Canal.

History This stretch of the Athy Branch of the Grand Canal, between Monasterevin and Athy, was constructed by the Grand Canal Company in 1785-91. Vicarstown Bridge is captioned on the 1839, 1889 and 1907 OS maps.

Component LAIAR-014-008 1

Type Bridge (road/canal) **Function** Inland waterway

Category Transport **Context** Infrastructure

Irish Grid 261510 200483 +/- 10m

Survey date 10/05/2007 **Surveyor** Fred Hamond

Remains Complete **Condition** Good **Current use** Road bridge

Description A masonry arch bridge carries a public road over the Athy Branch of Grand Canal. The abutments are of dressed masonry blocks, laid to courses. There are tow rope marks on the quoins of the west abutment. The arch is of segmental profile and spans 6.62m. Its voussoirs and soffit are of dressed stone blocks. The canal is 4.60m wide and the towpath through its west side is 2.02m. The spandrels are detailed as the abutments. The parapets are of squared random rubble, laid to courses and set 6.40m apart; they are coped with masonry blocks. There are outward curving squared random rubble wing walls at both ends; they terminate in square piers (missing at the north-east end). The approaches are ramped and the carriageway is curved.

Interest Architectural; Historical

Evaluation This bridge is of architectural merit as a typical canal bridge, and also of historical interest due to its canal association. It is of regional industrial heritage significance and merits its current inclusion in the Record of Protected Structures (LA 594).

Rating Regional **Protection** RPS LA 594 **Action** None (in RPS)

Associated with LAIAR-014-004

Other dbase LA Bridge R024; NIAH 12801403 (R)

References

Survey of the Architectural Heritage of County Laois (Dublin: NIAH, 2002). Page(s)

Waterways Ireland. Grand Canal Barrow Line: Vicarstown Bridge - Inventory and Condition Survey (Collins Engineers, 2007). Page(s)

LAIAR-014-008_01 FWH 10/05/2007 General view from north.



LAIAR-014-008_02 FWH 10/05/2007 North elevation.



LAIAR-014-008_03 FWH 10/05/2007 South elevation.



Site no LAIAR-014-010

Dunrally Bridge

County Laois; Kildare **Townland** Vicarstown (Dodd); Co Kildare **Town**

Planning Laois CC; Kildare CC **Discovery map** 55 **Six-inch map** 14

Summary A seven-arch masonry road bridge over the River Barrow. Built by James Grattan in 1820. Formerly had a toll house at the Laois end.

History A plaque on this bridge notes its construction by James Grattan, the local MP and owner of the Dunrally Estate, in 1820; he was the son of Henry Grattan. Dunrally Bridge is captioned on the 1839, 1889 and 1907 OS maps. According to Barry, a toll house once stood at the Laois end of the bridge. The concrete underpinning probably dates from the Barrow Drainage Works of c.1930.

Component LAIAR-014-010 1

Type Bridge (road/river) **Function** Road & pedestrian

Category Transport **Context** Infrastructure

Irish Grid 263648 201802 +/- 10m

Survey date 23/04/2007 **Surveyor** Fred Hamond

Remains Complete **Condition** Good **Current use** Road bridge

Description A seven-arched masonry bridge carries a public road over the River Barrow on the boundary with Co Kildare. Five of the spans are in normal use over the river, with a smaller flood arch on each bank. The abutments and piers are of random masonry blocks, brought to courses (the quoins of the abutments are unembellished). Rounded cutwaters rise to arch spring level at both ends of the piers. They are of dressed stone blocks, regularly-coursed, and are embellished around their tops with moulded string courses and curvilinear caps. The outside faces of the abutments are similarly decorated. The abutment and piers of the river arches are underpinned with concrete. All the spans are of segmental profile and have dressed stone voussoirs and squared rubble soffits. The river arches are wider and higher towards the middle of the river; from the left- to right bank, they measure 6.10m, 7.62m, 9.2m (estimated from road), 7.64m and 6.11m. The left and right-bank flood arches measure 3.80 and 3.66m respectively. The spandrels and parapets are of roughly-dressed masonry blocks, brought to courses; the latter are coped with concrete. In the middle of the road face of the upstream (north) parapet is a dressed stone plaque reading: "Built by/ James Grattan/ Esqr in the/ year 1820". Both parapets terminate in out-projecting piers; that on the downstream right bank has been damaged by a vehicle. A string course runs under the parapets and around the piers. The parapet and deck are both curved in elevation.

Interest Architectural; Historical; Setting

Evaluation This bridge is of architectural merit on account of its quality of construction, scale and detailing. It is also of historical interest due to its attested date and association with James Grattan. It also adds interest to the riverscape hereabouts. It is of national industrial heritage significance and merits its current inclusion in the Record of Protected Structures (LA 592).

Rating National **Protection** RPS LA 592 **Action** None (in RPS)

Associated with

Other dbase LA Bridge R025; NIAH 12801401 (R)

References

- Barry, M. Across Deep Waters: Bridges of Ireland (Dublin: Frankfort Press, 1985). Page(s) 93
- Craig, M. and Garner, W. Second Report on Areas and Sites of Historical Interest in County Laois (Dublin: An Foras Forbartha, 1976). Page(s) 89
- Duffy, J. Barrow Bridges (Tullow: John Duffy, 2007). Page(s) 110
- Hogan, T.L. 'River Barrow Drainage', in Proc Inst Civil Engineers Ireland, vol.65 (1939). Page(s)
- Survey of the Architectural Heritage of County Laois (Dublin: NIAH, 2002). Page(s)

LAIAR-014-010_01 FWH 23/04/2007 Upstream (SE) elevation.



LAIAR-014-010_02 FWH 23/04/2007 Downstream (SE) elevation.



LAIAR-014-010_03 FWH 23/04/2007 Cutwater detail on upstream side.



LAIAR-014-010_04 FWH 23/04/2007 Stonework detail at downstream left-bank end.



LAIAR-014-010_05 FWH 10/05/2007 Plaque on upstream parapet.



LAIAR-014-010_06 FWH 10/05/2007 Plaque on upstream parapet.



Site no LAIAR-014-014

Bob's Bridge

County	Laois	Townland	Brockley Park	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	14

Summary An arched masonry accommodation bridge of 1791 in the former grounds of Brockley Park. It once spanned the now-infilled tailrace to a flour mill (LAIAR-014-012) in Stradbally.

History This bridge was erected in 1791. It is cited as Bob's bridge on the 1839, 1889 and 1907 OS maps. It is shown on the 1839 map as spanning the tailrace to a flour mill in Stradbally (LAIAR-014-012). This section of the tailrace had been infilled by the 1889 map survey.

Component LAIAR-014-014 1

Type	Bridge (road/river)	Function	Road & pedestrian		
Category	Transport	Context	Settlement		
Irish Grid	257996 197107		+/- 10m		
Survey date	10/05/2007	Surveyor	Fred Hamond		
Remains	Substantial remains	Condition	Fair	Current use	Disused

Description An arched masonry bridge carries an accommodation track over a mill race in the former grounds of Brockley Park. It is of random rubble construction throughout. The arch is of segmental profile and spans 3.09m. Its voussoirs are of dressed stone blocks, with slightly accentuated keystones. The parapets and approach walls are now largely missing; the former were 3.02m apart. A dressed stone plaque on the road face of the north-east parapet reads "Bob's Bridge/ June 1791". The approaches are slightly ramped. The mill race is infilled to both sides.

Interest Historical

Evaluation This bridge is primarily of historical interest on account of its attested date of construction - 1791. It is of local industrial heritage significance.

Rating	Local	Protection		Action	None
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Associated with

Other dbase

References

LAIAR-014-014_01 FWH 10/05/2007 Upstream (SW) elevation.



LAIAR-014-014_02 FWH 10/05/2007 Plaque on downstream (NE) parapet.



Site no **LAIAR-014-015** **Farmyard Bridge**

County Laois **Townland** Brockley Park **Town**
Planning Laois CC **Discovery map** 55 **Six-inch map** 14

Summary A masonry arch bridge of 1775 or 1862 carries an accommodation track over the Stradbally River in the former grounds of Brockley Park.

History The bridge bears the dates 1775 and 1862. Whether it dates from 1775 or is an 1862 rebuild is uncertain. It is shown on the 1839 and 1889 OS maps and is captioned 'Farmyard Bridge' on the 1907 edition.

Component LAIAR-014-015 1

Type Bridge (road/river) Function Road & pedestrian
Category Transport Context Settlement
Irish Grid 258199 197080 +/- 10m
Survey date 10/05/2007 Surveyor Fred Hamond
Remains Complete Condition Good Current use Road bridge

Description A masonry arch bridge carries an accommodation track over the Stradbally River in the former grounds of Brockley Park. The abutments are of masonry blocks. The arch is of semi-elliptical profile and spans 7.36m. The voussoirs are of roughly dressed blocks. The upstream (west) one bears the inscription '1775 and the downstream one bears two indecipherable letters under which is the date 1862. The spandrels and parapets are of random and squared random rubble. A string course runs under the parapets which are 4.43m apart. The entire bridge is heavily overgrown with ivy.

Interest Historical

Evaluation This bridge is primarily of historical interest on account of its attested date - either 1775 or 1862. It is of local industrial heritage significance.

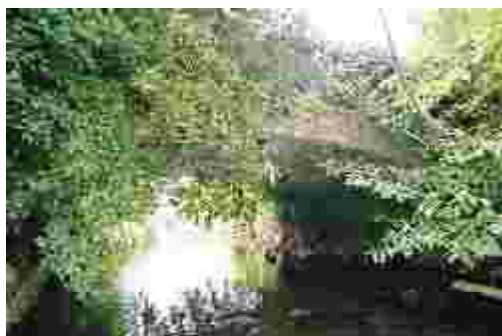
Rating Local **Protection** **Action** None

Associated with

Other dbase

References

LAIAR-014-015_01 FWH 10/05/2007 Downstream (E) elevation.



LAIAR-014-015_02 FWH 10/05/2007 Datestone on downstream voussoir.



Site no LAIAR-014-018

County	Laois	Townland	Ballykilcavan	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	14

Summary A mid 19th century tunnel carries an accommodation footpath under a public road, connecting Ballykilcavan House with its walled garden on the opposite side of the road.

History First shown on the 1888 OS map and cited as 'tunnel' on the 1907 edition. Its absence from the 1839 map indicates that it is of mid 19th century date.

Component LAIAR-014-018 1

Type	Bridge (road/foot)	Function	Road & pedestrian		
Category	Transport	Context	Settlement		
Irish Grid	259477 196718		+/- 10m		
Survey date	10/05/2007	Surveyor	Fred Hamond		
Remains	Complete	Condition	Good	Current use	Road bridge

Description An arched masonry tunnel carries an accommodation footpath under a public road, connecting Ballykilcavan House with a walled garden on the south-east side of the road. The abutments and arch are of brick. The latter is of segmental profile and spans 1.97m. Its voussoirs are of rusticated stone blocks. The north-west arch ring also has an additional facing of undressed sandstone. The spandrels are of random rubble. The parapets along the road are set in from the end of the tunnel and are also of random rubble; they have dressed copings and are 15.21m apart. The height of the tunnel is 2.03m. There is a wrought-iron gate at its south-east end.

Interest Architectural; Rarity

Evaluation This pedestrian tunnel is of some architectural interest on account of its rusticated arches. It is also one of only two bridges of its type (road/foot) in Co Laois (the other is LAIAR-029-028). It is of local industrial heritage significance.

Rating	Local	Protection		Action	None
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Associated with

Other dbase

References

LAIAR-014-018_01 FWH 10/05/2007 North-west elevation. Note rusticated arch ring.



LAIAR-014-018_02 FWH 10/05/2007 View inside tunnel, looking SW.



Site no LAIAR-014-019 **Curraclone Bridge**

County Laois **Townland** Inch (Stradbally By); Monaferrick **Town**
Planning Laois CC **Discovery map** 55 **Six-inch map** 14

Summary A triple-arch masonry road bridge of 18th/early 19th century date over the Stradbally River.

History Curraclone Bridge is captioned on the 1839, 1889 and 1907 OS maps.

Component LAIAR-014-019 1

Type Bridge (road/river) **Function** Road & pedestrian
Category Transport **Context** Infrastructure
Irish Grid 260264 198233 +/- 10m
Survey date 10/05/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge

Description A triple-arch masonry bridge carries a public road over the Stradbally River. It is of random rubble construction throughout except for the cutwaters on the up- and downstream ends of the piers. These are of angled profile and rise to arch spring level. They are replacements of the original dressed masonry ones, one of which partly survives at the upstream end of the right-bank pier. The arches are of segmental profile with dressed stone voussoirs. From the left to right bank they span 3.65m, 3.65m and 3.67m. The parapets are 6.10m apart. The bed of the river is pitched with stones.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase LA Bridge C117

References

LAIAR-014-019_01 FWH 10/05/2007 Upstream (S) elevation.



Site no LAIAR-014-022

County	Laois	Townland	Vicarstown (Cosby); Derrybrock	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	14

Summary Site of a culvert of c.1790 carrying a tributary of the Stradbally River under the Athy Branch of the Grand Canal. Probably survives in a buried state.

History This stretch of the Athy Branch of the Grand Canal, between Monasterevin and Athy, was constructed by the Grand Canal Company in 1785-91. This aqueduct is shown on the 1839, 1889 and 1907 OS maps; it is explicitly captioned on the 1907 edition.

Component LAIAR-014-022 1

Type	Bridge (canal/river)	Function	Inland waterway		
Category	Transport	Context	Infrastructure		
Irish Grid	261536 199481		+/- 10m		
Survey date	10/05/2007	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	A culvert carrying a tributary of the Stradbally River under the Athy Branch of the Grand Canal. It was inspected at its east end, but no traces could be found of the culvert or the watercourse. It probably exists in a buried state.				

Interest None

Evaluation No industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with LAIAR-014-004

Other dbase

References

Site no LAIAR-014-023 **Derrybrock Bridge**

County Laois **Townland** Ballymanus; Derrybrock **Town**
Planning Laois CC **Discovery map** 55 **Six-inch map** 14

Summary Site of an 18th/early 19th century road bridge over the Stradbally River. by-passed by a reinforced-concrete bridge a short distance downstream in the later 1900s.

History Derrybrock Bridge is captioned on the 1839, 1889 and 1907 OS maps. The present bridge is a later 20th century construction over a realigned section of the road just east of its previous line.

Component LAIAR-014-023 1

Type Bridge (road/river) Function Road & pedestrian
Category Transport Context Infrastructure
Irish Grid 261375 199096 +/- 10m
Survey date 10/05/2007 Surveyor Fred Hamond
Remains No visible remains Condition N/A Current use N/A
Description Superseded by the present single-span concrete bridge (component 2).

Component LAIAR-014-023 2

Type Bridge (road/river) Function Road & pedestrian
Category Transport Context Infrastructure
Irish Grid 261399 199085 +/- 10m
Survey date 10/05/2007 Surveyor Fred Hamond
Remains Complete Condition Good Current use Road bridge
Description A reinforced-concrete bridge carries a public road over the Stradbally River on a realigned section of road. It is of concrete construction throughout, with a slab deck spanning 9.67m. The parapets comprise four-bar tubular steel railings spaced at 8.41m. They terminate in square concrete piers with pyramidal caps.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase LA Bridge C116

References

Craig, M. and Garner, W. Second Report on Areas and Sites of Historical Interest in County Laois (Dublin: An Foras Forbartha, 1976).

Page(s) 86

LAIAR-014-023_01 FWH 10/05/2007 Upstream (NW) elevation.



Site no LAIAR-014-024 **Camac Aqueduct**

County Laois **Townland** Ballymanus; Derrybrock **Town**
Planning Laois CC **Discovery map** 55 **Six-inch map** 14

Summary A triple-arched masonry aqueduct of c.1790 carries the Athy Branch of the Grand Canal over the Stradbally River.

History This stretch of the Athy Branch of the Grand Canal, between Monasterevin and Athy, was constructed by the Grand Canal Company in 1785-91. 'Camac Aqueduct' is captioned on the 1839, 1889 and 1907 OS maps.

Component LAIAR-014-024 1

Type Bridge (canal/river) **Function** Inland waterway
Category Transport **Context** Infrastructure
Irish Grid 261505 199028 +/- 10m
Survey date 10/05/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Aqueduct

Description A triple-arched masonry bridge carries the Athy Branch of the Grand Canal over the Stradbally River. The abutments and piers are of dressed and coursed masonry blocks. There are semicircular cutwaters rising to arch spring level at both ends of the cutwaters. The arches are of segmental profile; from the left- to right bank, they span 6.06m, 6.09m and 6.06m. Their voussoirs are of vee-joined dressed stone blocks and their soffits of masonry blocks. There is a chamfered string course through the arches at spring level; these continue around the tops of the cutwaters. The spandrels and parapets are detailed as the abutments. They are embellished with shallow rounded pilasters which rise from the abutments and cutwaters to the tops of the parapets. The parapets, which are coped with concrete, have outward curving ends which terminate in circular out-projecting piers. A chamfered string course runs along the base of each parapet and around the pilasters and piers. The parapets are 12.10m apart; the canal is 4.94m wide at this point. There is a metalled road along the east side of the canal and an unsurfaced track on the opposite side. There are slots for stop planks at both ends of the aqueduct. A weathered dressed-stone plaque on the canal face of the upstream (west) parapet reads 'Camac Aqueduct'. A concrete curb has been added to the east side of the bank as a road safety measure. Vegetation growth has displaced some of the masonry on the upstream pilasters.

Interest Architectural; Historical; Setting

Evaluation This aqueduct is of architectural interest on account of its quality of construction and scale. It is also of historical interest due to its association with the Grand Canal and enhances the landscape hereabouts. It is the most prominent aqueduct on the Co Laois section of the Athy Canal. It is of national industrial heritage significance and merits its current inclusion in the Record of Protected Structures (LA 603).

Rating National **Protection** RPS LA 603 **Action** None (in RPS)

Associated with LAIAR-014-004

Other dbase NIAH 12801414 (R)

References

- Craig, M. and Garner, W. Second Report on Areas and Sites of Historical Interest in County Laois (Dublin: An Foras Forbartha, 1976). Page(s) 86
- Survey of the Architectural Heritage of County Laois (Dublin: NIAH, 2002). Page(s)
- Waterways Ireland. Grand Canal Barrow Line: Camac Aqueduct - Inventory and Conditon Survey (Collins Engineers, 2007). Page(s)

LAIAR-014-024_01 FWH 10/05/2007 Upstream (W) elevation.



LAIAR-014-024_02 FWH 10/05/2007 Downstream (E) elevation, from NE.



LAIAR-014-024_03 FWH 10/05/2007 Vegetation displacement on upstream pilaster.



LAIAR-014-024_04 FWH 10/05/2007 Plaque on west parapet.



LAIAR-014-024_05 FWH 10/05/2007 Looking south over aqueduct from north end.



Site no **LAIAR-014-025** **Ballymanus Bridge**

County Laois **Townland** Ballymanus **Town**
Planning Laois CC **Discovery map** 55 **Six-inch map** 14

Summary An arched masonry accommodation bridge, built c.1790, over the Athy Branch of the Grand Canal.

History This stretch of the Athy Branch of the Grand Canal, between Monasterevin and Athy, was constructed by the Grand Canal Company in 1785-91. Ballymanus Bridge is captioned on the 1839, 1889 and 1907 OS maps.

Component **LAIAR-014-025 1**

Type Bridge (road/canal) **Function** Inland waterway
Category Transport **Context** Infrastructure
Irish Grid 261595 198542 +/- 10m
Survey date 10/05/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Fair **Current use** Road bridge
Description A masonry arch bridge carries an unsurfaced accommodation track over the Athy Branch of Grand Canal. The abutments are of roughly dressed masonry blocks. There are tow rope marks on the quoins of the west abutment. The arch is of segmental profile and spans 6.61m. Its voussoirs are of dressed stone blocks and the soffit of random rubble. The canal is 4.55m wide and the towpath through its west side is 2.06m. The spandrels and parapets are of random rubble. The latter are coped with roughly dressed masonry blocks and are 3.16m apart. There are outward curving random rubble wing walls at both ends which terminate in square piers. The approaches are ramped and the carriageway is humped.

Interest Architectural; Historical

Evaluation This bridge is of architectural merit and also of historical interest due to its association with the Grand Canal. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.

Rating Regional **Protection** **Action** RPS

Associated with

Other dbase

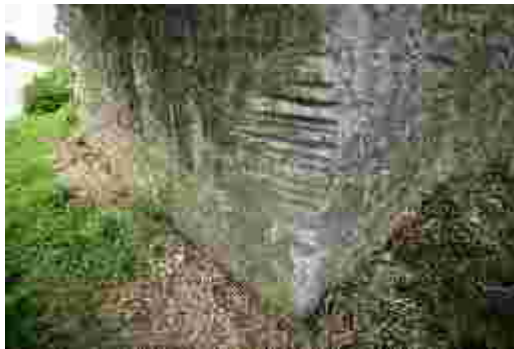
References

Delany, R. and Addis, J. Guide to the Barrow: Lowtown to St Mullins (Dublin: Inland Waterways Association of Ireland, 1977). Page(s) 11
Waterways Ireland. Grand Canal Barrow Line: Ballymanus Bridge - Inventory and Condition Survey (Collins Engineers, 2007). Page(s)

LAIAR-014-025_01 FWH 10/05/2007 South elevation.



LAIAR-014-025_02 FWH 10/05/2007 Tow rope marks at north end of west abutment.



Site no LAIAR-014-028

County	Laois; Carlow	Townland	Bawn; Co Carlow	Town	
Planning	Laois CC; Carlow CC	Discovery map	55	Six-inch map	14

Summary Site of a culvert of c.1790 carrying the Blackford Stream under the Athy Branch of the Grand Canal. Probably survives in a buried state.

History This stretch of the Athy Branch of the Grand Canal, between Monasterevin and Athy, was constructed by the Grand Canal Company in 1785-91. A tunnel is captioned on the 1839 and 1889 OS maps, and an aqueduct on the 1907 edition.

Component LAIAR-014-028 1

Type	Bridge (canal/river)	Function	Inland waterway		
Category	Transport	Context	Infrastructure		
Irish Grid	262456 197592 +/- 10m				
Survey date	10/05/2007	Surveyor	Fred Hamond		
Remains	No visible remains	Condition	N/A	Current use	N/A
Description	A culvert carrying a tributary of the Blackford Stream under the Athy Branch of the Grand Canal. It was inspected at its east end, but no traces could be found of the culvert or the watercourse. It probably exists in a buried state.				

Interest None

Evaluation No industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with LAIAR-014-004

Other dbase

References

Site no LAIAR-014-029**James's Bridge**

County	Laois	Townland	Brockley Park	Town	
Planning	Laois CC	Discovery map	55	Six-inch map	14

Summary Site of 18th/early 19th century accommodation bridge over Stradbally River in former grounds of Brockley Park. Replaced in mid 20th century by a metal beam bridge a short distance upstream.

History James's Bridge is cited on the 1839, 1889 and 1907 OS maps. It was replaced in the mid 20th century by the present accommodation bridge a short distance upstream.

Component LAIAR-014-029 1

Type	Bridge (road/river)	Function	Road & pedestrian
Category	Transport	Context	Settlement
Irish Grid	257794 196633		+/- 10m
Survey date	10/05/2007	Surveyor	Fred Hamond
Remains	No visible remains	Condition	N/A
		Current use	N/A
Description	Site of accommodation bridge over Stradbally River in the former grounds of Brockley Park. Superseded by component 2.		

Component LAIAR-014-029 2

Type	Bridge (road/river)	Function	Road & pedestrian
Category	Transport	Context	Settlement
Irish Grid	257762 196620		+/- 10m
Survey date	10/05/2007	Surveyor	Fred Hamond
Remains	Complete	Condition	Good
		Current use	Road bridge
Description	A skew metal beam bridge carries an accommodation track over the Stradbally River. The abutments are of mass concrete. The span comprises four RSJs - two of 30cm x 12.5cm cross section, and two of 36cm x 15cm section. They span 6.09m measured orthogonally, and 6.61m on the skew. The deck comprises wooden planks laid transversely over the beams. The parapets are four-bar tubular steel railings secured to brackets affixed to the outside faces of the outer RSJs. They are 2.75m apart and terminate in RSJ posts. There is a gate across the north-west end of the bridge.		

Interest None

Evaluation No industrial heritage significance.

Rating	Record only	Protection		Action	None
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Associated with

Other dbase

References

LAIAR-014-029_01 FWH 10/05/2007 Downstream (NE) elevation.



Site no LAIAR-014-031 **Sheean Bridge; {Shane Bridge}**

County Laois **Townland** Bawn **Town**
Planning Laois CC **Discovery map** 55 **Six-inch map** 14

Summary A skew arched masonry culvert of 18th/early 19th century date carries the Blackford Stream under a public road at the boundary with Co Kildare.

History Shane Bridge is captioned on the 1839 and 1889 OS maps, and Sheean Bridge on the 1907 edition.

Component LAIAR-014-031 1

Type Bridge (road/river) **Function** Road & pedestrian
Category Transport **Context** Infrastructure
Irish Grid 262330 196950 +/- 10m
Survey date 10/05/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge

Description A skew arched masonry culvert carries a tributary of the Blackford Stream under a public road on the boundary with Co Kildare. It is of random rubble construction throughout. The arch is of semicircular profile and has roughly dressed voussoirs; it spans 1.80m (measured orthogonally). The downstream (north-east) parapet has been rebuilt in split random rubble.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase

References

LAIAR-014-031_01 FWH 10/05/2007 Downstream (NE) elevation.



Site no **LAIAR-014-032** **Thomas's Bridge**

County Laois **Townland** Brockley Park **Town**
Planning Laois CC **Discovery map** 55 **Six-inch map** 14

Summary Site of 18th/early 19th century accommodation bridge over tailrace to flour mill (LAIAR-014-012) in grounds of Brockley Park.

History Thomas's Bridge is captioned on the 1839 OS map. It is not cited or shown on the 1889 or 1907 editions, the tailrace which it spanned having been infilled.

Component **LAIAR-014-032 1**

Type Bridge (road/river) Function Road & pedestrian
Category Transport Context Settlement
Irish Grid 257671 196769 +/- 10m
Survey date 10/05/2007 Surveyor Fred Hamond
Remains No visible remains Condition N/A Current use N/A
Description Site of accommodation bridge over tailrace to flour mill (LAIAR-014-012) in the former grounds of Brockley Park.

Interest None

Evaluation No industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase

References

Site no **LAIAR-014-038** **Ballykilcavan Bridge**

County Laois **Townland** Ballykilcavan; Garrans; Mill-land
Planning Laois CC **Discovery map** 55 **Six-inch map** 14

Summary A five-arch masonry road bridge of 1713 over the Stradbally River. Pedestrian refuges on upstream side and also three flood arches.

History This bridge was erected in 1713. Hunt Walsh, landlord of the adjoining Ballykilcavan Estate, is noted on a plaque as its instigator and Arthur Collk as its overseer. It is shown on the 1839, 1889 and 1907 OS maps.

Component LAIAR-014-038 1

Type Bridge (road/river) **Function** Road & pedestrian
Category Transport **Context** Infrastructure
Irish Grid 259609 197176 +/- 10m
Survey date 10/05/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge

Description A five-arch masonry bridge carries a public road over the Stradbally River. It is of random rubble construction throughout. The right-bank arch is dry. There are triangular cutwaters to the upstream (south) ends of the piers; they are of squared random rubble and rise to the arch crowns. There are no cutwaters on the downstream side. The three arches towards the left-bank side are of flattish segmental profile and the two on the right are semicircular; from the left to right bank they span 3.07m, 3.02m, 3.05m, 3.05m and 3.04m. The piers are relatively thick relative to the width of the spans. Angled pedestrian refuges rise from the upstream piers. Set into the second refuge from the right-bank end is a plaque bearing the inscription (in places difficult to read) "Hunt Walsh/ Esq Arthur/ Collk Gent/ over 1713". There may also be an inscription along the bottom, now indecipherable. The parapets are 3.95m apart. The bed of the river is pitched with stones. The parapets on the approach from the east are partly raised in order to follow the contour of an earthen bank meets the road at right angles on both sides. Set into the eastern approach are two semicircular flood arches of 1.03m and 1.25m span. At the extreme east end is a third such arch, set at an angle and of 1.58m span.

Interest Architectural; Historical

Evaluation This bridge displays a number of architectural traits typical of 18th century bridges - unembellished random rubble construction, a large number of small arches, relatively thick piers, narrow carriageway, and pedestrian refuges. It is also of historical interest having an attested date of 1713 and named builder. It is of regional industrial heritage interest and merits its current inclusion in the Record of Protected Structures (LA 599).

Rating Regional **Protection** RPS LA 599 **Action** None (in RPS)

Associated with

Other dbase LA Bridge C118; NIAH 12801409 (R)

References

Craig, M. and Garner, W. Second Report on Areas and Sites of Historical Interest in County Laois (Dublin: An Foras Forbartha, 1976). Page(s) 86

Survey of the Architectural Heritage of County Laois (Dublin: NIAH, 2002). Page(s)

LAIAR-014-038_01 FWH 10/05/2007 Upstream (S) elevation.



LAIAR-014-038_02 FWH 10/05/2007 Cutwater and refuge detail on upstream side, from right bank.



LAIAR-014-038_03 FWH 10/05/2007 Downstream (N) elevation.



LAIAR-014-038_04 FWH 10/05/2007 Looking west along carriageway. Note raised parapets and refuges.



LAIAR-014-038_05 FWH 10/05/2007 Plaque set into pedestrian refuge.



LAIAR-014-038_06 FWH 10/05/2007 North elevation of flood arches on east approach.



Site no LAIAR-014-041

County Laois **Townland** Coolroe; Rosnamullane **Town**
Planning Laois CC **Discovery map** 55 **Six-inch map** 14

Summary A twin-arched masonry road bridge of 18th/early 19th century date over the Glash River.

History A bridge is shown on the 1839, 1889 and 1907 OS maps.

Component LAIAR-014-041 1

Type Bridge (road/river) **Function** Road & pedestrian
Category Transport **Context** Infrastructure
Irish Grid 261020 202377 +/- 10m
Survey date 10/05/2007 **Surveyor** Fred Hamond
Remains Complete **Condition** Good **Current use** Road bridge
Description A twin-arched masonry span carries a public road over the Glash River. It is of random rubble construction throughout. There is an angled cutwater to the upstream (west) side, rising to arch spring level. The arches are of semicircular profile. The left-bank one spans 1.54m and the right one 1.75m. The parapets are 6.41m apart.

Interest None

Evaluation No special industrial heritage significance.

Rating Record only **Protection** **Action** None

Associated with

Other dbase

References

LAIAR-014-041_01 FWH 10/05/2007 West elevation.



